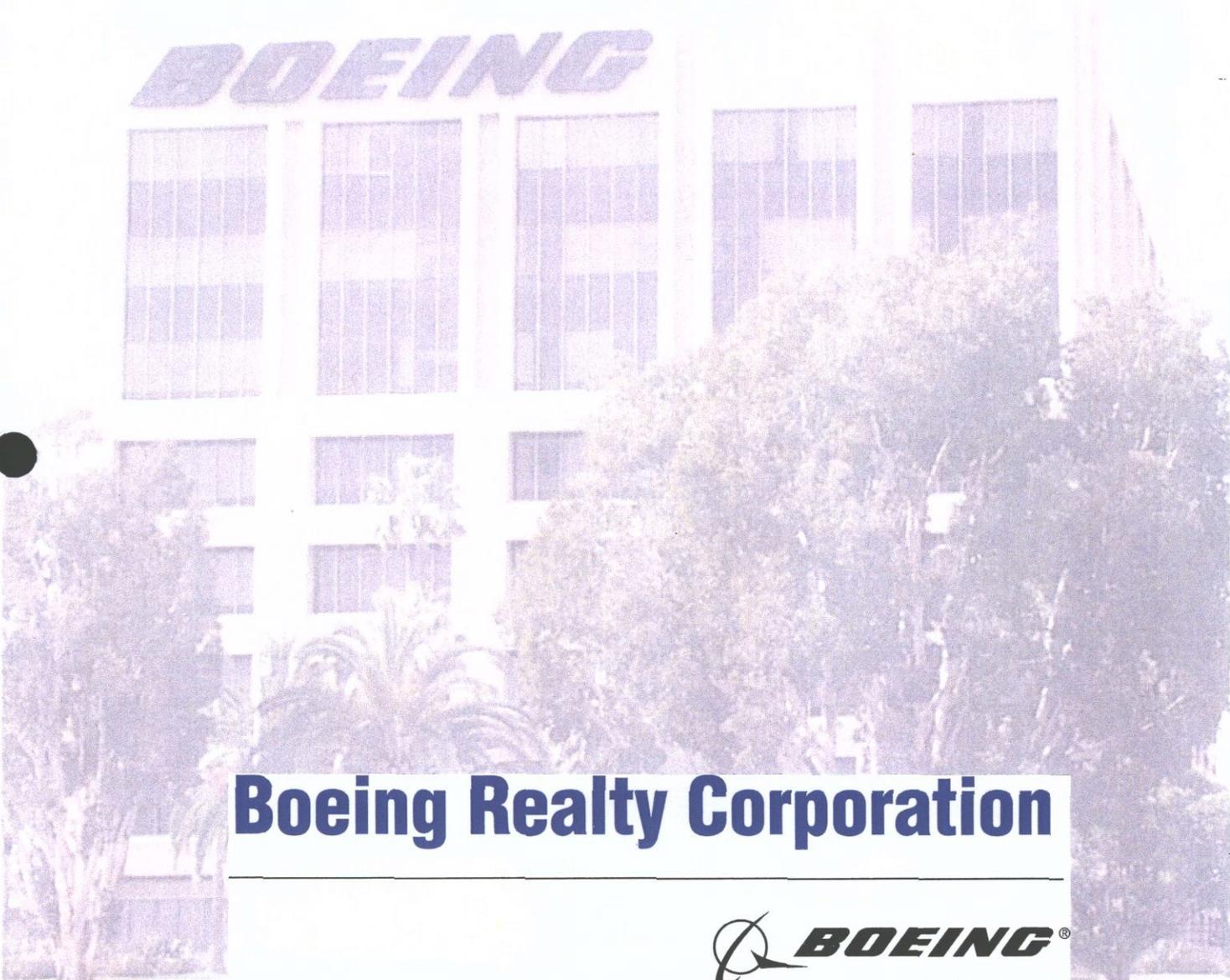


BOEING

INTEGRATED DEFENSE SYSTEMS

SPECIFIC PLAN

SEAL BEACH SITE



BOEING

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SEAL BEACH
CALIFORNIA

AUGUST 2003

BOEING INTEGRATED DEFENSE SYSTEMS SPECIFIC PLAN

SEAL BEACH SITE

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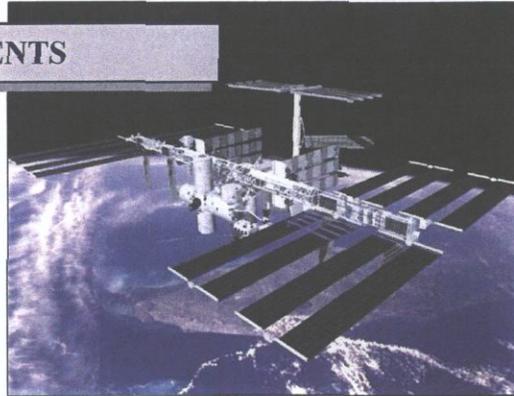
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BOEING INTEGRATED DEFENSE SYSTEMS SPECIFIC PLAN

SEAL BEACH SITE

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**BOEING INTEGRATED DEFENSE SYSTEMS SPECIFIC PLAN
SEAL BEACH SITE**

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BOEING INTEGRATED DEFENSE SYSTEMS SPECIFIC PLAN
SEAL BEACH SITE

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International Space Station

Photographs Courtesy of NASA and The Boeing Company

Architectural images and landscape themes graphically presented in this Specific Plan are concepts to provide general direction and are not intended to dictate a particular style or vernacular.

Illustrative plans and site plans presented in this Specific Plan are concepts intended to depict a hypothetical development scenario for the property.

The final location and size of all proposed public utilities and facilities will be determined in connection with final approval of Vesting Tentative Tract Map 16375.

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**SECTION ONE
INTRODUCTION AND SUMMARY**

**BOEING
INTEGRATED DEFENSE SYSTEMS
SPECIFIC PLAN
SEAL BEACH SITE**

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SECTION ONE

INTRODUCTION AND PROJECT SUMMARY

1.1 PURPOSE AND INTENT

The Boeing Integrated Defense Systems (BIDS) Specific Plan document provides assurances to the City of Seal Beach that land uses and new development within the Specific Plan area will be consistent with the policies, goals and objectives of the General Plan. The BIDS Specific Plan establishes the planning concept, design theme, development regulations and administrative procedures necessary to achieve compatible, orderly and efficient development of the site.

The BIDS Specific Plan has been prepared under the direction of Boeing Realty Corporation (BRC), a wholly-owned subsidiary of The Boeing Company.

The purpose of the BIDS Specific Plan project is based on the following underlying principle:

To create a quality business park campus compatible with existing Boeing Integrated Defense Systems campus, operations and facilities.

The BIDS Specific Plan establishes the following:

- ☑ Identification, location, character and intensities of the planned business park, hotel and commercial development activities.
- ☑ Alignment and design of the circulation system.
- ☑ Identification and location of all public and private facilities and infrastructure necessary to facilitate the project.
- ☑ Land use and development standards for the existing Boeing Space & Communications facilities and for the planned business park, hotel and retail uses.
- ☑ A compatible design theme for the project area, creating continuity within the planned business park and compatibility with existing buildings, facilities, operations and other surrounding land uses.



In 1993, Boeing was selected as the prime contractor for the International Space Station. Boeing is responsible for design and development, testing, evaluation, hardware and software.

1.2 AUTHORITY AND PROCEDURE

The BIDS Specific Plan has been prepared and established under the authority granted to the City of Seal Beach by the State of California under the California Government Code, Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457. Specific Plans are intended to be a bridge between the local General Plan and individual development proposals. Under the authority of the above code sections, the State of California encourages cities to adopt Specific Plans either by resolution to establish a policy document or by ordinance to establish a regulatory document. The BIDS Specific Plan is intended as a regulatory document and is subject to Planning Commission review and City Council adoption by ordinance. When adopted by City legislative action, this Specific Plan document will serve planning, policy and regulatory functions. The BIDS Specific Plan contains standards,

procedures and guidelines to accomplish this purpose.



Boeing's Integrated Defense Systems operates in Seal Beach. Building 80, above, is one of the main buildings on the Seal Beach campus.

FIGURE 1-1

The BIDS Specific Plan is also authorized by the City Council adoption of Ordinance 1193, which created the Specific Plan Regulation Zone and procedures for implementation and adoption of a Specific Plan. The Code of the City of Seal Beach provides for Ordinance 1193 in Article 17, Specific Plan Regulation Zone and Article 29.5, Specific Plans. The BIDS Specific Plan is consistent with the regulations and procedures as adopted by the City of Seal Beach.

Adoption or amendment of a Specific Plan constitutes a project under the California Environmental Quality Act (CEQA) and the State's Environmental Impact Report (EIR) Guidelines. An Environmental Impact Report (EIR 02-1) with identified mitigation measures has been prepared as a companion report to the Specific Plan.

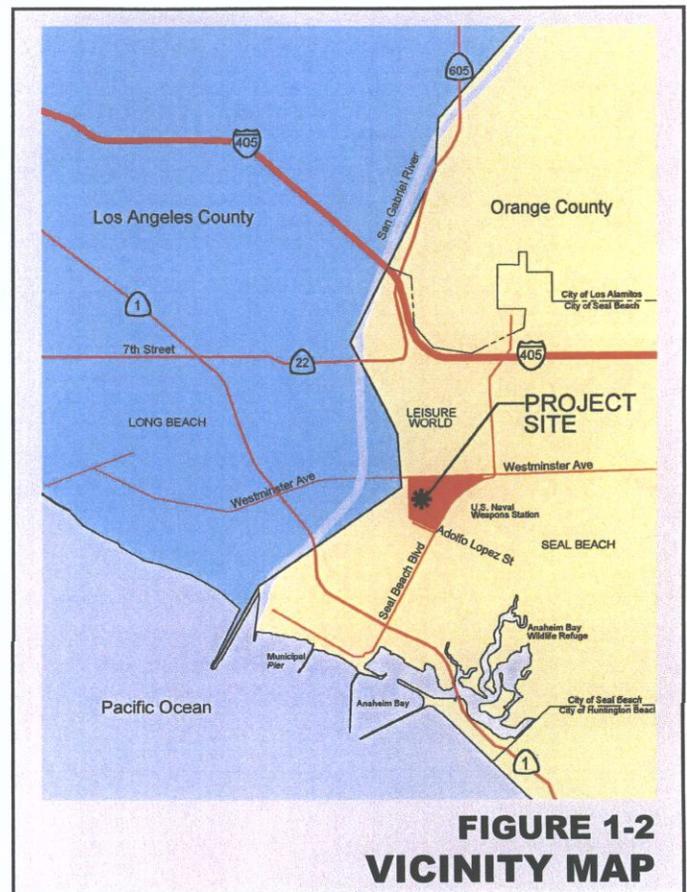
All development plans, subdivision maps, site plans and other entitlement requests for properties located within the boundaries of this BIDS Specific Plan area shall be consistent with the regulations set forth in this Specific Plan.

1.3 LOCAL AND REGIONAL CONTEXT

The BIDS Specific Plan covers a 107-acre project area located in the City of Seal Beach in the westernmost portion of Orange County, California (Figure 1-2). Both primary and arterial roadways are adjacent to the site, providing both regional and local access within the immediate area. A mix of developed land uses surround the property, including industrial, commercial, residential, flood control and military uses. Table 1-1 identifies the land uses adjacent to the site.

The BIDS Specific Plan project area is accessed by Seal Beach Boulevard and Westminster Avenue (arterial and primary highways, respectively), through the City of Seal Beach and the City of Long Beach. The project is also located in close proximity to the California freeway system. Freeways in the immediate vicinity include:

- ☑ The San Diego Freeway (405) is located approximately 1 mile to the north and 2 miles to the northwest.
- ☑ The San Gabriel Freeway (605) is located approximately 2 miles to the northwest
- ☑ The Garden Grove Freeway (22) is located approximately 1.5 miles to the northeast.



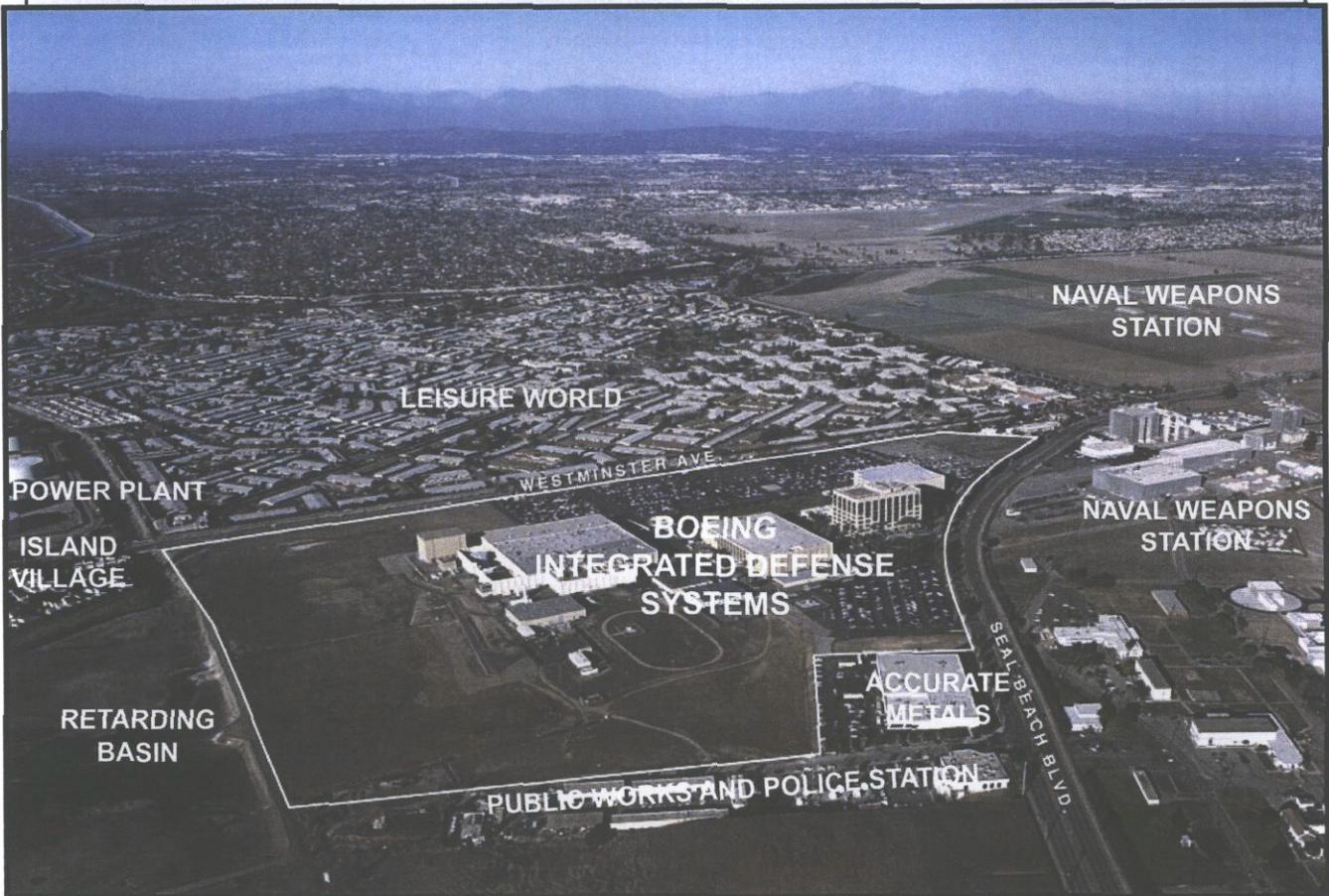
Pacific Coast Highway (1), the primary arterial along the coast, is located approximately 1 mile to the south and 1 mile to the west.

1.4 RELATIONSHIP OF THE BIDS SPECIFIC PLAN TO THE GENERAL PLAN

The City of Seal Beach General Plan identifies the 107-acre BIDS Specific Plan area for Light Industrial land uses. The site has been designated for industrial land uses since the mid-1960's, when North American Aviation initially developed the site. The existing General Plan recommends a "well-planned industrial park offering fully serviced, protected sites for quality industry." The BIDS Specific Plan meets the intent of the General Plan in that a quality business park is proposed for the property. However, the BIDS Specific Plan does require a General Plan Amendment for the following reasons:

- ☑ To amend the Land Use Element to specifically recommend that a Specific Plan be implemented for the BIDS property.
- ☑ To specifically permit point of sale industrial/manufacturing businesses in all Planning Areas and hotel and commercial uses in Planning Area 4.
- ☑ To change the General Plan designation from Light Industrial to Specific Plan Regulation (S.P.R.), thereby requiring the preparation of the BIDS Specific Plan.

TABLE 1-1 SURROUNDING LAND USES	
North:	Westminster Avenue, Leisure World Retirement Community
Northeast:	Strip Commercial Center, Westminster Avenue, Seal Beach Center, Seal Beach Boulevard, & the Seal Beach Naval Weapons Station
South:	City Police Station, City Public Works Yard, various City facilities, Animal Care Center
Southeast:	Accurate Metals, Seal Beach Boulevard & the Seal Beach Naval Weapons Station
East:	Seal Beach Boulevard & the Seal Beach Naval Weapons Station
West:	Los Alamitos Retarding Basin & Island Village Residential Community



**FIGURE 1-3
AERIAL PHOTOGRAPH**

1.5 ZONING REGULATIONS

The 107-acre BIDS Specific Plan area is presently zoned Light Manufacturing (M-1). The purpose of this designation as stated in the Code of the City of Seal Beach is as follows:

“It is the intent of the City to encourage and require all development in the M-1 zone to take place under an industrial park concept in order to insure compatibility with surrounding land use and to preserve the general health, safety and welfare of the community.”

The BIDS Specific Plan is proposed under the business park planning concept with the Specific Plan as the mechanism or tool that will assure compatibility, preserve the general health, safety and welfare of the community and create a cohesive, quality design for the project.

The adoption of the BIDS Specific Plan will supersede the existing zoning and establish a new set of development regulations and design guidelines for the 107-acre site. The zoning designation will change from the existing M-1 designation to S.P.R., which the BIDS Specific Plan will implement.



Building 90 on the Seal Beach campus. **FIGURE 1-4**

The BIDS Specific Plan permits opportunities for point-of-sale industrial/ manufacturing uses and allows for office, commercial, hotel and other land uses currently not identified in the M-1 zone. The BIDS Specific Plan also identifies a range of conditionally permitted land uses that are compatible with BIDS operations and the proposed business park. The BIDS Specific Plan further incorporates design guidelines and development regulations that will assure development compatibility with adjacent land uses.

1.6 SPECIFIC PLAN OBJECTIVES

Project objectives have been established for the development of properties within the BIDS Specific Plan. These objectives are identified below:

Planning and Environmental Design

- Provide for comprehensive land use and infrastructure planning, while respecting the physical and existing BIDS operational constraints of the site.
- Create a cohesive identity for the Specific Plan area, and provide a consistent project theme, development standards and design guidelines that allow design flexibility to better respond to market needs.
- Promote high quality design and well ordered spatial relationships among buildings and land uses.

- Unify the Specific Plan area through the implementation of a consistent landscape, architectural and street scene program; and create a comprehensive signage system that includes identification, directional and informational signage, which is appropriate for both the Specific Plan area and the overall community.



FIGURE 1-5

Business Park Building Concept. C & D Aerospace at the McDonnell Center Business Park.

industrial, manufacturing, research and development, commercial and office land uses in close proximity to similar existing uses.

Fiscal Benefits

- Allow business park uses that provide point of sale opportunities, and provide for a commercial planning area (Planning Area 4) allowing for hotel and retail land uses.

- Establish an efficient pattern of local circulation, both vehicular and pedestrian, which will also provide linkage between the project area and the surrounding community.

- Encourage revitalization and reuse within the project area in a logical, systematic manner, compatible with existing BIDS operations.

- Provide for water quality treatment of urban runoff for new development.

1.7 PROJECT OVERVIEW

A comprehensive land use plan has been prepared for the 107-acre BIDS Specific Plan area. This plan provides for development of a business park, combined with hotel and commercial uses, within the framework of the existing Boeing Integrated Defense Systems operations.

Land Use and Development

- Implement the policies of the Seal Beach General Plan for the project site and vicinity through the Specific Plan process.
- Increase connectivity within the Specific Plan area, and provide a planning framework that responds to the physical and market driven aspects of future development opportunities.
- Encourage the expansion of a range of employment opportunities within the City of Seal Beach by combining light

BIDS currently has 1,150,000 square feet of floor space, several parking areas, and other facilities on the developed portions of the campus. A summary description of each Planning Area and land use designation follows, with additional details provided in Section 2, Land Use Plan.

Planning Area 1: Existing Boeing Integrated Defense Systems Campus

Planning Area 1 (PA-1) is approximately 41 acres and consists of the core BIDS campus. This planning area is designated for the continuation of the BIDS facilities with requirements not to exceed BIDS's existing campus-wide floor area of 1,150,000 square feet. PA-1 currently supports 805,000 square feet of gross floor area. Therefore, up to 345,000 square feet of new gross floor area may developed within PA-1. The existing office, research and development, manufacturing, warehouse and distribution, and support operations land uses in this Planning Area will be maintained.

Boeing Integrated Defense Systems, a division of The Boeing Company, is located at the property. The property was acquired in 1996 as part of Boeing's acquisition of Rockwell International.

Planning Area 2: Business park

Planning Area 2 (PA-2) is approximately 16 acres encompassing a range of existing BIDS buildings and facilities. The existing uses will be preserved or replaced with new buildings and facilities provided for in the BIDS Specific Plan. Although this area is planned to be integrated into the new business park with new buildings, BIDS may require some or all of the existing buildings and facilities to be maintained. PA-2 currently supports 345,000 square feet of gross floor area and is designated for up to 345,000 square feet of new and/or existing gross floor area.

Planning Area 3: Business park

Planning Area 3 (PA-3) consists of approximately 45 acres of BIDS's vacant land and is planned for business park uses. The existing light industrial (M-1 zoning) and permitted uses will be preserved and expanded, as provided for in the BIDS Specific Plan. Two man-made drainage ditches will be incorporated into the landscape and water quality plan for the project in this area. PA-3 will provide for up to 628,000 square feet of new gross floor area.



**FIGURE 1-6
BUSINESS PARK BUILDING
CONCEPT**

The Konica Corporation at the McDonnell Center Business Park in Huntington Beach.

McDonnell Center Business Park was planned and developed by Boeing Realty Corporation

TABLE 1-2
BIDS SPECIFIC PLAN
EXISTING FACILITIES
(BY PLANNING AREA)

Planning Area	Description	Existing Building Area (square feet)
1	Core Boeing Integrated Defense Systems Campus, including buildings 80, 81, 82, 90, 92 and other facilities and structures	805,000
2	Various buildings and facilities including Buildings 84, 85, 86, 89, 91, 93, 94 and 100	345,000
3	Vacant lands	0
4	Unused parking lot	0
EXISTING FACILITIES ON 107 ACRES:		1,150,000 square feet

TABLE 1-3
BIDS SPECIFIC PLAN
PROPOSED LAND USES
(BY PLANNING AREA)

Planning Area	Land Use	Gross Acres	Rooms	Maximum Gross Floor Area (sq. ft.)	Description
1	Existing Light Industrial	41		1,150,000	Existing core BIDS facilities. Allows for up to 345,000 square feet of additional building area within PA-1.
2	Planned Business Park	16		345,000	Some or all of the existing buildings may be maintained and/or re-used. New business park buildings planned for PA-2 would require relocation and/or demolition of existing buildings and facilities.
3	Planned Business Park	45		628,000	Vacant land planned for business park buildings, roads, infrastructure, etc. Portions of the drainage ditches will be used for water quality treatment purposes.
4	Planned Hotel Commercial	5	120	55,000 32,500	Planned hotel and commercial land uses, including retail, restaurant and similar commercial uses. Business park uses are also permitted.
SPECIFIC PLAN TOTAL		107 acres	120 rooms	2,210,500	

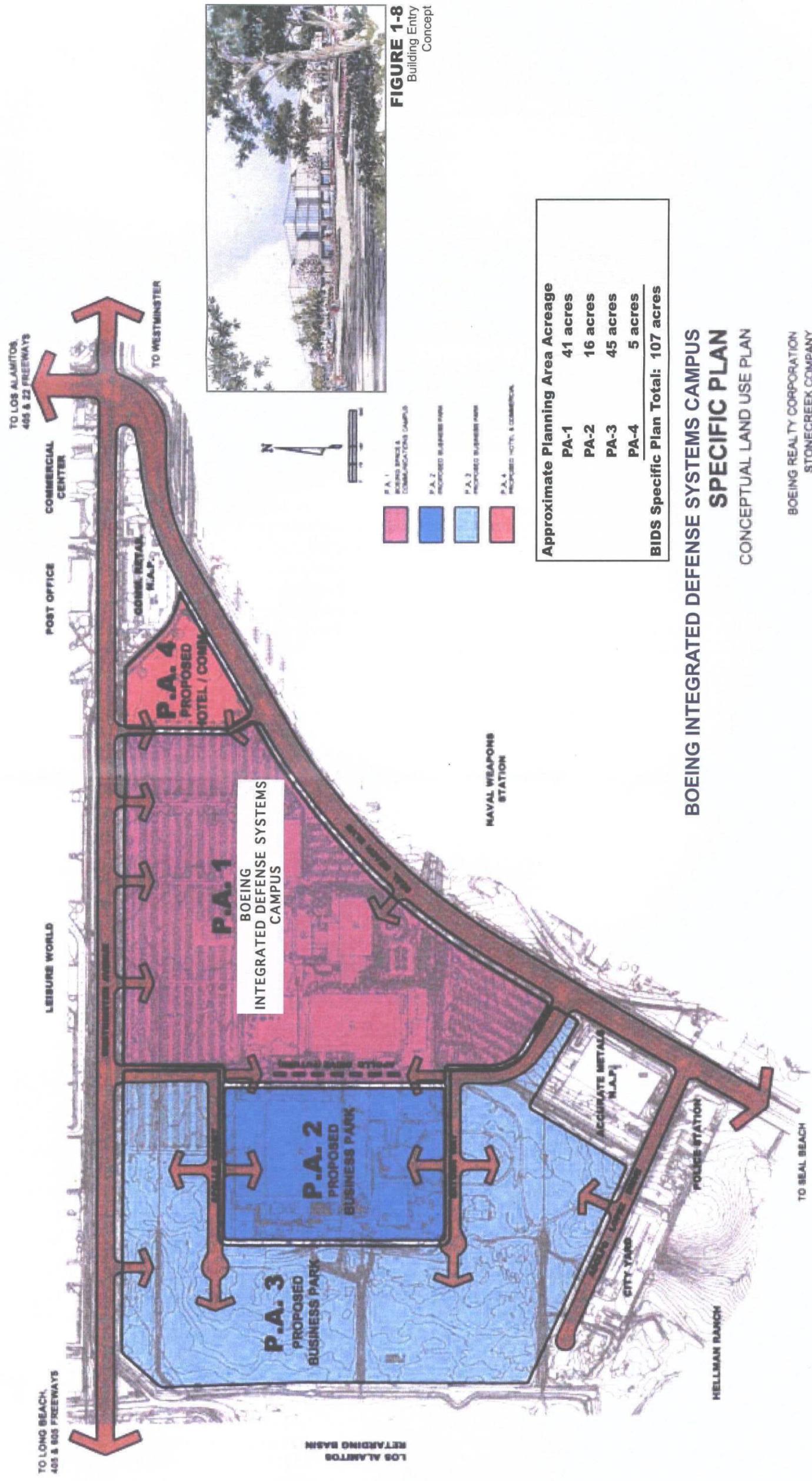


FIGURE 1-8
Building Entry
Concept

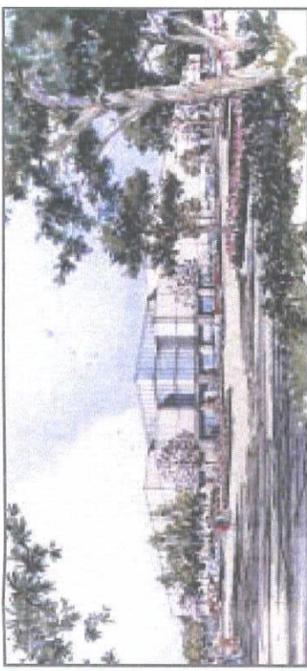


FIGURE 1-7
LAND USE PLAN

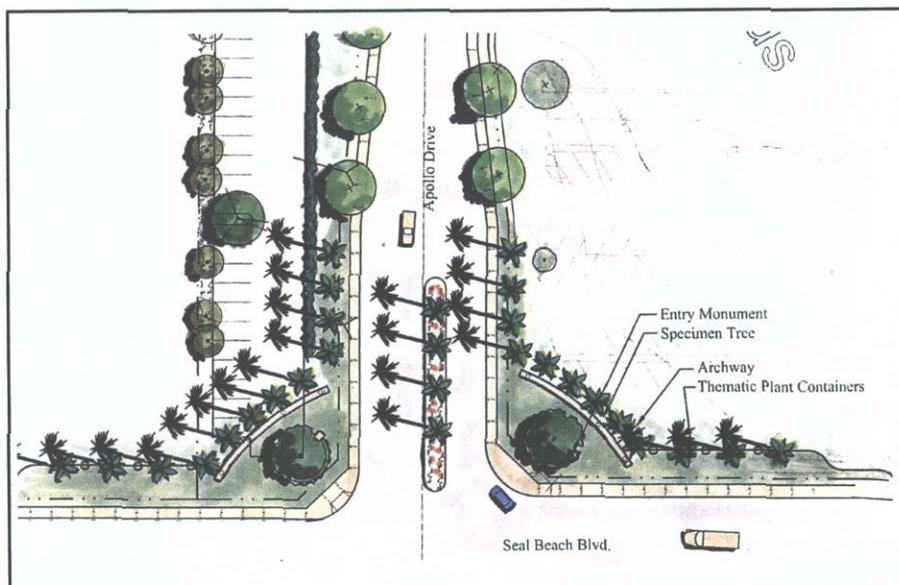
Planning Area 4: Hotel/Commercial Area

Planning Area 4 (PA-4) is approximately 5 acres and consists primarily of a (unused) parking lot at the eastern end of the property, between Westminster Avenue and Seal Beach Boulevard. This area will be developed with hotel and/or commercial uses, allowing for up to 120 hotel rooms and 32,500 square feet of new commercial building floor area. Business park uses are also permitted in PA-4, if market or other factors determine that hotel/commercial uses are not feasible for this site.

Circulation

The on-site circulation system has been designed to incorporate existing traffic signals to the extent feasible. Access to the proposed business park will occur from both Seal Beach Boulevard and Westminster Avenue. Both of these locations are developed with existing traffic signals and provide access to the existing BIDS campus. Apollo Drive will extend into the business park and may ultimately connect Seal Beach Boulevard to Westminster Avenue, if certain BIDS buildings are demolished.

Two roadways from Apollo Drive (Saturn Way and Apollo Court), will provide access to all new parcels. Both roads will cul-de-sac, providing safe and efficient on-site circulation for day-to-day traffic and emergency vehicles.



**FIGURE 1-9
PROPOSED APOLLO DRIVE ENTRANCE
AT SEAL BEACH BOULEVARD**

1.8 THE CALIFORNIA COASTAL ACT

The City of Seal Beach by Resolution Number 3248, approved and adopted a Draft Local Coastal Land Use Plan (LUP) on February 28, 1983, after determining it to be in accordance with the goals and objectives of the California Coastal Act. The draft LUP was conditionally approved by the California Coastal Commission in 1983, but has never been certified. The draft LUP from 1983 divides the City into seven planning areas. The BIDS Specific Plan project site is known in the draft LUP as Area 3, the Hellman-Rockwell Property.

Westminster Avenue, Seal Beach Boulevard, the San Gabriel River and the Marina

Hill District bound the Hellman/Rockwell Area, as designated in the draft LUP. This 336-acre site is currently under the control of six interests, including the City of Seal Beach Redevelopment Agency (Police Station and Public Works Facilities), Hellman Properties (Open Space, Oil Production and Residential Development), State Lands Commission (Designated for Visitor-Serving Uses), Orange County Flood Control District (Los Alamitos Retarding Basin), Accurate Metals (manufacturing) and The Boeing Company (existing headquarters campus for Boeing Space & Communications Group). With the exception of Accurate Metals, adjacent to the Boeing campus, the BIDS Specific Plan area is the only industrial development in the City, currently providing approximately 2,500 jobs at the Seal Beach site.

The BIDS Specific Plan is consistent with the Draft LUP in that it:

- Provides development of non-noxious industry, such as commercial, research and development and light-industry uses, in an area where such activities will be compatible with existing land uses.
- Provides commercial uses that will minimize the use of coastal access roads.
- Provides development contiguous with existing developed areas that will not have significant adverse effects on coastal resources.

Because the City's draft LUP and Local Coastal Program (LCP) remain un-certified, development within the BIDS Specific Plan area will require Coastal Development Permit (CDP) approval from the California Coastal Commission (CCC). The CCC is required to make findings that development of this site is in compliance with the goals and policies of the California Coastal Act of 1976 ("Coastal Act"). The CDP entitlement process with the CCC will be initiated after the City of Seal Beach discretionary approvals have been granted (i.e., General Plan Amendment, Zone Change, Vesting Tentative Tract Map and Precise Plan).

The BIDS Specific Plan includes objectives, design features and other components that are consistent with Coastal Act goals and policies. These include:

- Water quality control features to treat urban runoff.
- Maintenance and enhancement of two man-made drainage ditches for water quality and retention purposes.
- Biological surveys indicate that there are no Environmentally Sensitive Habitat Areas (ESHAs) located on the property.
- Visitor-serving commercial land uses proposed near the corner of Seal Beach Boulevard and Westminster Avenue.

- Public pedestrian access and connection to the Hellman Ranch open space proposed with improvements to Adolfo Lopez Drive.
- Industrial development located contiguous to existing development and primary arterial highways adjacent to the property.

It is the intent of this BIDS Specific Plan to identify land uses and incorporate design features so that development will be consistent with the Coastal Act.

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**SECTION TWO
LAND USE PLAN**

**BOEING
INTEGRATED DEFENSE SYSTEMS
SPECIFIC PLAN
SEAL BEACH SITE**

Boeing Realty Corporation



SECTION TWO

LAND USE PLAN

2.1 DEVELOPMENT CONCEPT

The BIDS Specific Plan development concept provides for a planned mixed-use business park development, that will be compatible with existing Boeing Space & Communications facilities and operations at the site. The proposed BIDS Specific Plan land uses are the product of extensive

alternatives analysis and assessments. The Specific Plan establishes the general type, location, parameters and character of all development within the site's boundaries, while allowing for creative design ideas on individual projects consistent with an overall concept.



**FIGURE 2-1
BUSINESS PARK
BUILDING CONCEPT**

The BIDS Specific Plan takes into account the entire existing 107-acre BIDS Property, southwest of the intersection of Westminster Avenue and Seal Beach Boulevard, in the City of Seal Beach, County of Orange. The property was acquired through The Boeing Company's acquisition of Rockwell International in 1996, and is home to BIDS's headquarters operations.

The 107-acre site contains approximately 1,150,000 square feet of office, research and development, manufacturing, and support operations space. There are nine major structures, seven minor facilities/support structures and other related facilities on the property. The property also provides approximately 3,100 parking spaces. Approximately 41 acres of the site are currently vacant lands. Additional new development, new activities, re-use or restructuring of the existing Boeing Space & Communications facilities can be accommodated by the regulations of this Specific Plan.

The Specific Plan is designed to allow for development in a manner that is compatible with the surrounding area within, and adjacent to, the City of Seal Beach. The property's existing features and proximity to regional transportation systems make the site ideal for a variety of compatible business park land uses, and for expansion within the main BIDS campus. The development concept has been designed to be compatible with the existing site and current surrounding land uses, and is consistent with the community's desire for a strong, self-sufficient economy to attract and maintain jobs and create a more diverse economic base for the City of Seal Beach.

2.2 SPECIFIC PLANNING AREAS

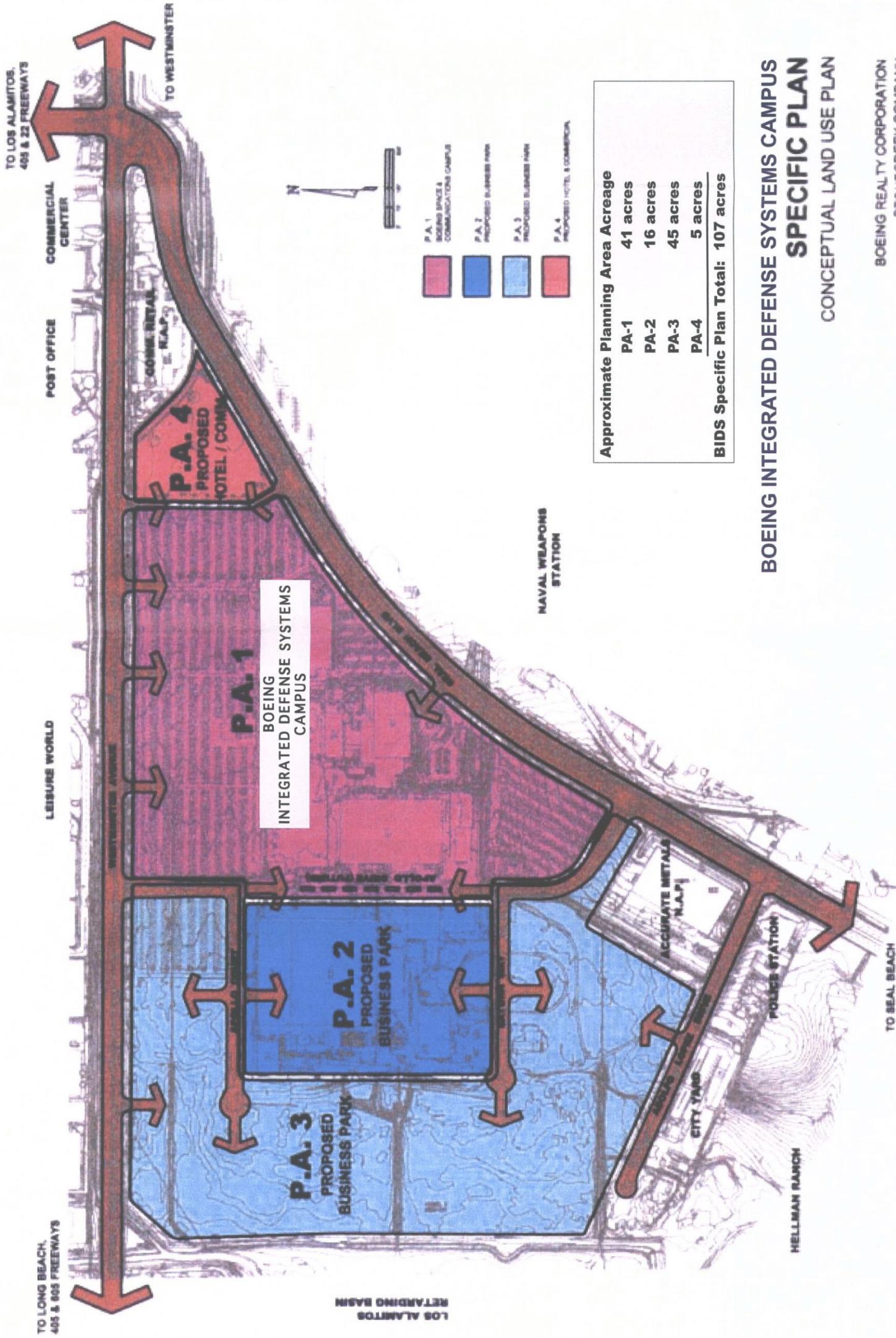
The BIDS Specific Plan is divided into four (4) Planning Areas:

- 1) Business Park
- 2) Business Park
- 3) Business Park
- 4) Hotel/Commercial

The proposed land uses and Planning Areas are illustrated in Figure 2-2 and detailed in Table 2-1, below.

TABLE 2-1 BOEING INTEGRATED DEFENSE SYSTEMS SPECIFIC PLAN LAND USES					
PLANNING AREA	LAND USE	GROSS ACRES	MAXIMUM GROSS FLOOR AREA/F.A.R.	ROOMS	FUNCTION/ CHARACTERISTICS
1	Business Park	41	1,150,000 Max.: F.A.R.: 0.75		Existing core campus of Boeing Integrated Defense Systems. Mix of uses including manufacturing, office, research and development, light industrial, warehouse and distribution, and support services. Existing building area represents 805,000 square feet. Additional building area of up to 345,000 square feet is permitted.
2	Business Park	16	345,000 Max.: F.A.R.: 0.60		Existing buildings and facilities may be reused or maintained for current BSC operations. New development of this PA would require relocation of existing facilities and demolition of existing buildings. Business park uses include manufacturing, light industrial, research and development, warehouse and distribution, and support services.
3	Business Park	45	628,000 Max.: F.A.R.: 0.60		This PA consists of a parking area and vacant land and is designated for the majority of the new business park. New roads and infrastructure are also proposed. Uses include manufacturing, light industrial, research and development and warehouse and distribution, and support services. Portions of the drainage ditches will be utilized for water quality treatment purposes.
4	Hotel/ Commercial/ Business Park	5	87,500 Max.: F.A.R.: 0.65	120	Planned hotel and commercial land uses, including retail, restaurant and similar commercial uses. Business park uses are also permitted.
TOTAL		107	2,210,500	120	

Notes: (1) Square footage may vary and may transfer between Planning Areas if intensity of use is consistent with the "Trip Generation Budget" for the BIDS Specific Plan and allowed within the Planning Areas.
 (2) F.A.R. indicated is maximum for each Planning Area.
 (3) See Table 5-1, page 5-5 for permitted uses, and Table 5-2, page 5-6 for development standards.



**BOEING INTEGRATED DEFENSE SYSTEMS CAMPUS
SPECIFIC PLAN
CONCEPTUAL LAND USE PLAN**

BOEING REALTY CORPORATION
STONECREEK COMPANY
DE REVERE & ASSOCIATES

**FIGURE 2-2
LAND USE PLAN**

2.3 PLANNING AREA 1

BUSINESS PARK (EXISTING BOEING SPACE & COMMUNICATIONS CAMPUS)

PA-1 is approximately 41 acres and comprises the existing core campus use of the property. This includes Buildings 80, 81, 82, and 90, the surface parking fronting Westminster Avenue (also known as parking lots 3-6), and other facilities and structures. The existing office, research and development, warehouse and distribution, manufacturing, and support operations land uses in this Planning Area will be preserved and maintained. PA-1 currently supports 805,000 square feet of gross floor area. To respond to potential government and business contracts, developing communication and space technologies, and to support general business demands, PA-1 is designated for a maximum of 1,150,000 square feet of gross floor area (an additional 345,000 square feet). The maximum F.A.R. for PA-1 is 0.75.

2.4 PLANNING AREA 2

PROPOSED BUSINESS PARK

PA-2 is approximately 16 acres and includes a range of BIDS facilities and buildings, including Buildings 84, 85, 86, 91, 93, 94, 96, and 100, a fire system tank farm, and an electrical substation. The existing gross floor area is approximately 345,000 square feet. Existing buildings and facilities in PA-2 may be maintained, reused, relocated or eliminated, depending on BIDS business requirements. PA-2 is designated for business park purposes with a maximum of 345,000 square feet of new and/or existing gross floor area. The maximum F.A.R. for PA-2 is 0.60.

2.5 PLANNING AREA 3

PROPOSED BUSINESS PARK

PA-3 is approximately 45 acres of vacant land. This Planning Area provides for the majority of new development on the BIDS site. New light industrial buildings with up to 628,000 square feet of gross floor area are designated for this Planning Area. The maximum F.A.R. for PA-3 is 0.60.

PA-3 will include a new road system including ingress/egress from Seal Beach Boulevard and Westminster Avenue via Apollo Drive, providing access to the new industrial park and existing facilities.



**FIGURE 2-3
BUSINESS PARK
BUILDING CONCEPT**

Two man-made drainage ditches will be incorporated into the water quality and landscape plan for the project in this area. The water quality concept is to incorporate bio-filtration, treatment areas and best management practices (BMPs) to capture low flows and provide retention to meet the stormwater runoff and water quality objectives of local and state agencies.

2.6 PLANNING AREA 4

PROPOSED HOTEL / COMMERCIAL AREA

PA-4 is approximately 5 acres consisting primarily of (unused) parking lot 7 at the



**FIGURE 2-4
HOTEL CONCEPT**

eastern end of the property, between Westminster Avenue and Seal Beach Boulevard. This area is planned for hotel and/or commercial uses, allowing for up to 120-hotel rooms and 32,500 square feet of commercial, including retail, restaurant and/or

other similar commercial uses. Business park uses are also permitted in this Planning Area.

2.7 LAND USE POLICIES

2.7.1 The existing Boeing Space & Communications facilities may expand within each Planning Area after Precise Plan Review and/or Subdivision Map approval.

2.7.2 Office uses shall be allowed in all Planning Areas.

2.7.3 Point of sale industrial business and manufacturing uses are permitted and encouraged in all Planning Areas.

2.7.4 Business park-related land uses, particularly light manufacturing, assembly, research and development, warehouse and distribution, are permitted in all Planning Areas.

2.7.5 Planning Area 4 is intended for hotel/commercial uses. Commercially reasonable efforts to market the property for this use shall continue for a period of 36 months after issuance of a Coastal Development Permit. If, after this 36-month period, hotel/commercial uses are determined not to be feasible, Planning Area 4 may be developed with business park land uses.

2.8 PHASING CONCEPT

Development starts and occupancy is not mandated by any phasing schedule. Development will be dictated by market demand and phased accordingly. The conceptual phasing schedule identified below is a best estimate for planning purposes only.

- Demolition, grading and construction of the roadways and required infrastructure improvements to serve Planning Areas 2 and 3 of the project are anticipated to begin by the end of 2003, after all entitlements have been issued. These improvements would be completed by mid-2004 in a single phase.
- Building construction may commence by early 2004 for the portion of Planning Area 3 located adjacent to Westminster Avenue and continue in a southerly progression.
- Building construction in Planning Area 4 may commence by early 2005.
- Occupancy is anticipated to occur as early as mid-2004, with full occupancy expected by the end of 2006.

2.9 CIRCULATION PLAN

Access to the City of Seal Beach and The BIDS Specific Plan area is provided by Seal Beach Boulevard and Westminster Avenue (arterial and primary streets, respectively), through the City of Seal Beach and the City of Long Beach. The City's General Plan designates Seal Beach Boulevard as a Major Arterial Highway with a 120-foot right-of-way. Westminster Avenue is designated as a Primary Street with a 100-foot right-of-way.

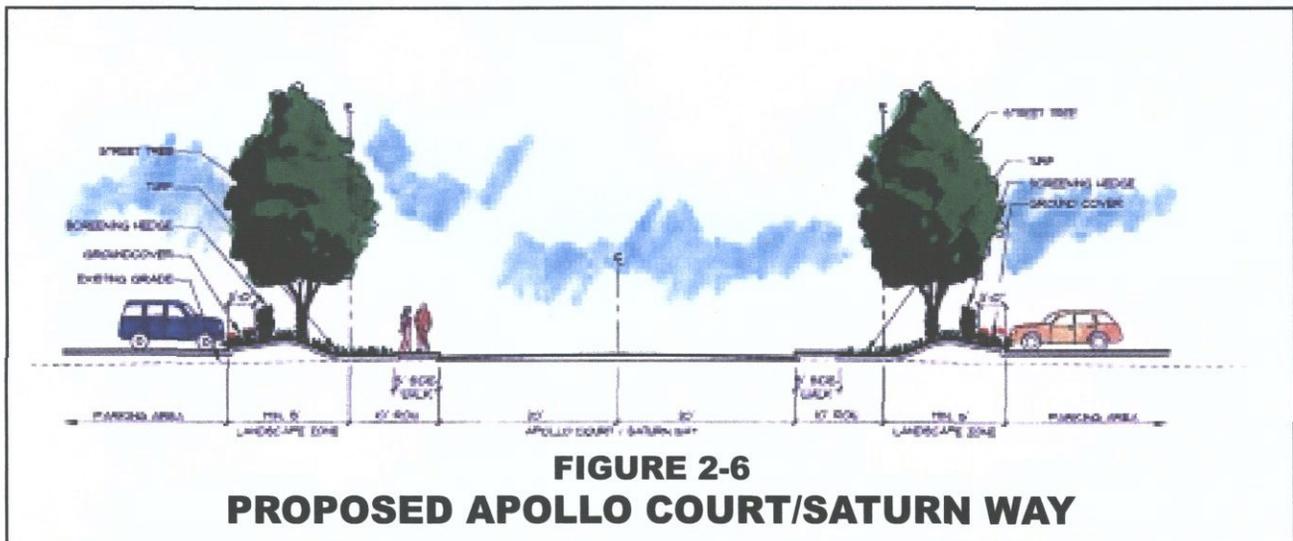
The project is also located in close proximity to the California freeway system. Freeways in the immediate vicinity include:

- ◆ The San Diego Freeway (405) located approximately 1 mile to the north and 2 miles to the northwest.
- ◆ The San Gabriel Freeway (605) located approximately 2 miles to the northwest.
- ◆ The Garden Grove Freeway (22) located approximately 1.5 miles to the northeast.
- ◆ Pacific Coast Highway (1), the primary arterial along the coast, located approximately 1 mile to the south.

The Land Use Plan and Illustrative Site Plan illustrate the general alignments and locations for streets within the BIDS Specific Plan.

Internal circulation for the existing campus is currently provided by a network of private streets and drives that meet the requirements of current BIDS operations at the site. New primary access locations into the project area will be from Westminster Avenue and Seal Beach Boulevard (Apollo Drive). Apollo Drive may ultimately connect Seal Beach Boulevard and Westminster Avenue, with the demolition of buildings and facilities in Planning Area 2. Apollo Drive access points have been located and designed to provide full turning movements at existing signalized intersections. Two roads from Apollo Drive (Saturn Way and Apollo Court) will provide access to the new parcels and will cul-de-sac. The roadway system will adequately serve the projected traffic volumes for the Specific Plan area.

The circulation system will accommodate the build-out of the Specific Plan area. Initial street construction and future phased construction or reconstruction will be

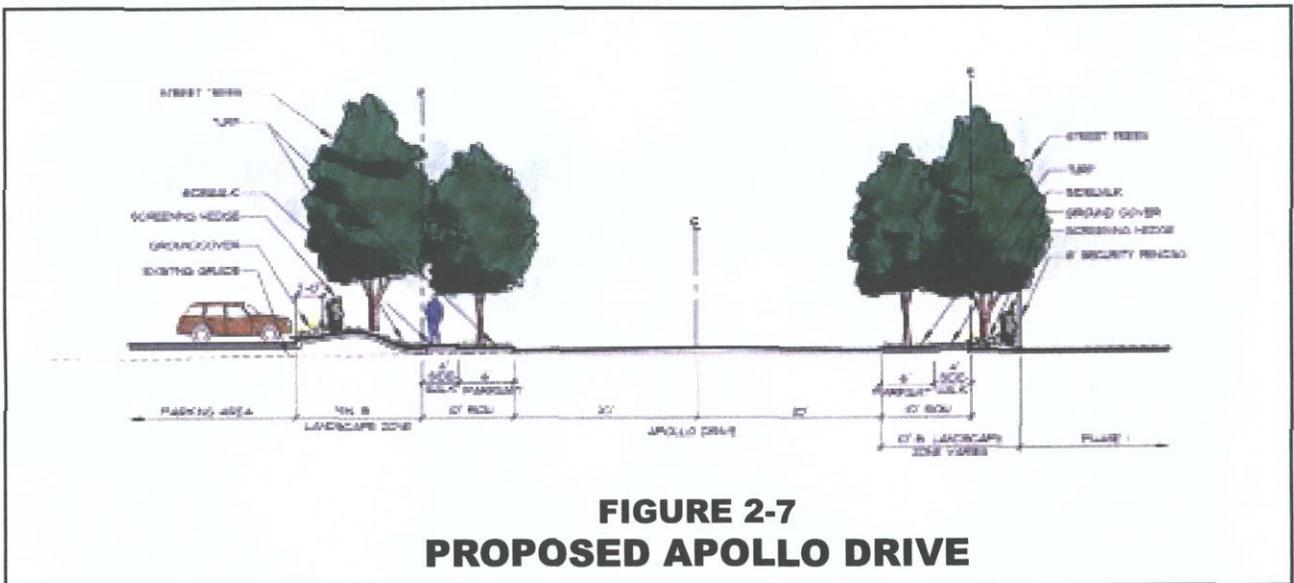


**FIGURE 2-6
PROPOSED APOLLO COURT/SATURN WAY**

completed in advance of occupancy of new facility-phased construction. The Director of Development Services and the Director of Public Works shall approve phasing plans for street improvement construction, consistent with development construction phasing and implemented through the Precise Plan Review and/or Subdivision Map process.

2.10 CIRCULATION POLICIES

- 2.10.1 Saturn Way and Apollo Court are proposed to accommodate a public right-of-way of 60 feet.
- 2.10.2 Upon development of Planning Area 2, and in coordination with the City Engineer, Apollo Drive may be extended to connect Westminster Avenue with Seal Beach Boulevard.



**FIGURE 2-7
PROPOSED APOLLO DRIVE**

- 2.10.3 Apollo Drive is proposed to accommodate a public right-of-way of 60 feet.
- 2.10.4 Final right-of-way design for public streets within the BIDS Specific Plan area will be determined in connection with final approval of Vesting Tentative Tract Map 16375.
- 2.10.5 Project entries may include a larger cross-section to accommodate full turning movements, medians and traffic signals as determined by the Director of Public Works.
- 2.10.6 Pedestrian sidewalks shall be incorporated into the right-of-way for Apollo Drive, Apollo Court and Saturn Way. Sidewalks shall be installed to best accommodate pedestrian needs adjacent to new development within the Specific Plan area.
- 2.10.7 Other existing ingress/egress to Westminster Avenue and Seal Beach Boulevard to the BIDS campus shall be maintained.
- 2.10.8 Adolfo Lopez Drive shall be improved adjacent to the project. Access to the adjacent lot(s) from Adolfo Lopez Drive shall be permitted.
- 2.10.9 Landscaped parkways for public streets are permitted between the curb and sidewalk.
- 2.10.10 On street parking shall not be permitted anywhere in the project area.
- 2.10.11 The "Trip Generation Budget" identified in E.I.R. 02-1 shall be reviewed with each individual project request at the time of Precise Plan Review and/or Subdivision Map submittal.

2.11 GRADING CONCEPT

Site grading will require cut and fill to create building pads that are slightly elevated to match existing, developed conditions, and to facilitate adequate sewerage and drainage. Final grading plans will be approved by the City Engineer, prior to issuance of a grading permit.

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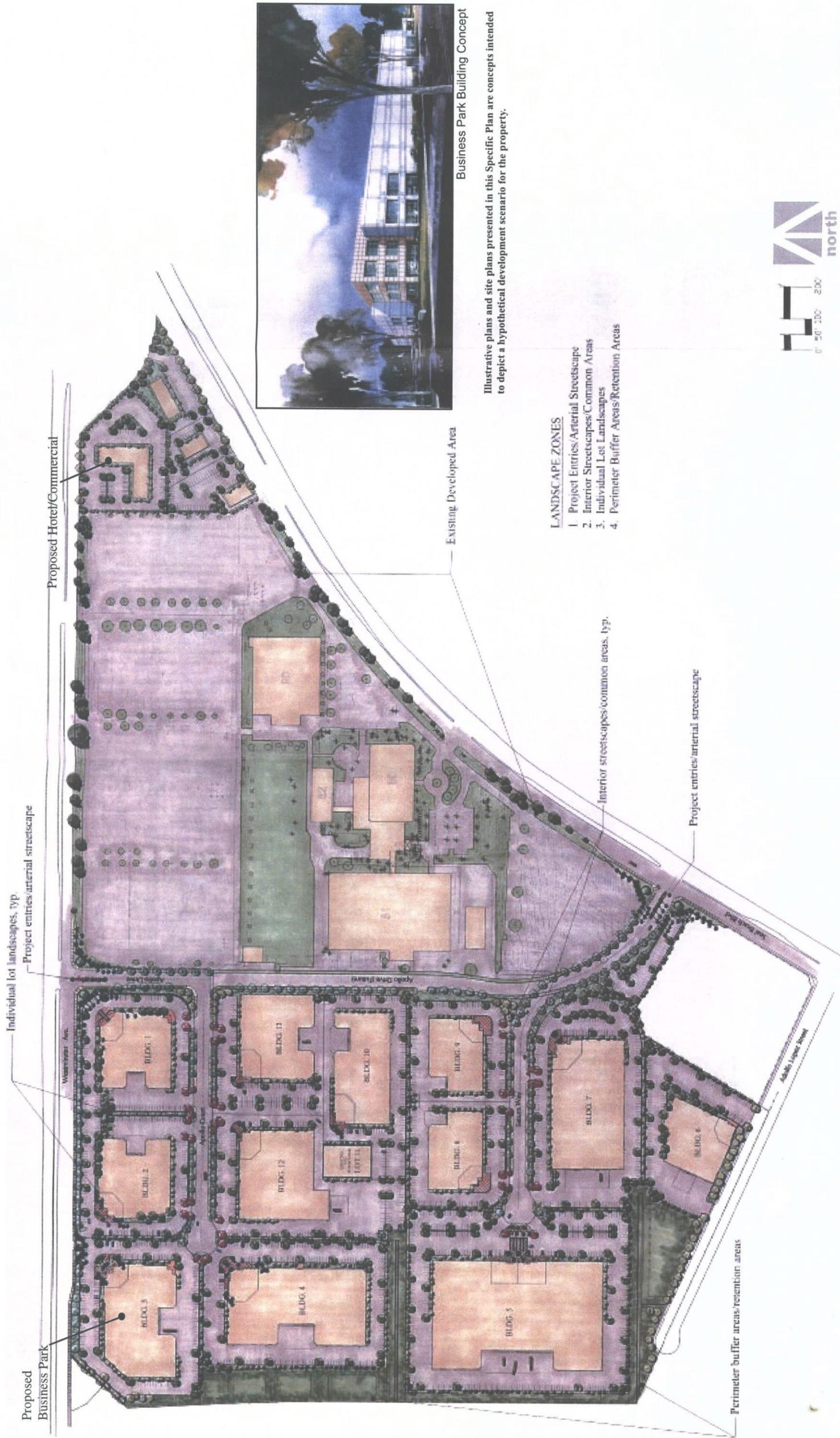
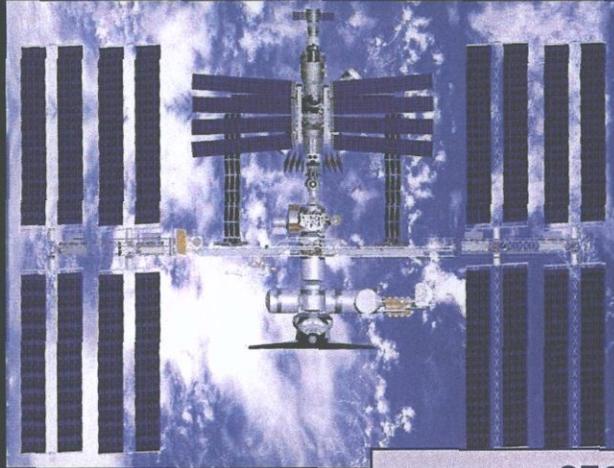


FIGURE 2-5
ILLUSTRATIVE SITE PLAN



**SECTION THREE
PUBLIC FACILITIES AND SERVICES**

**BOEING
INTEGRATED DEFENSE SYSTEMS
SPECIFIC PLAN
SEAL BEACH SITE**

Boeing Realty Corporation



SECTION THREE

PUBLIC FACILITIES AND SERVICES

The public facilities plans identify existing and proposed infrastructure, storm drain, sewer and water facility improvements to serve development within the Specific Plan area. Existing private utilities and facilities within the BIDS campus will continue to be the responsibility of the property owner. The sizing and location of public facilities identified in this section of the Specific Plan have been estimated for planning purposes only and are subject to refinements in the final design of the project. The final design is subject to the approval of the City Engineer.

3.1 WATER SYSTEM PLAN

An adequate water supply is necessary to serve new development within the Specific Plan area. The City of Seal Beach Public Works Department currently provides domestic water service to the property. Existing water lines and facilities will be extended and new facilities will be constructed with new development (Figure 3-2, Water and Sewer Facilities Plan).

Currently, the City of Seal Beach provides water to its service area from four wells that have a total pumping capacity of 11.46 million gallons per day (mgd) or 7,800 gallons per minute (gpm). Additionally, the City of Seal Beach owns approximately 3.9 mgd (2,700 gpm) capacity in the Metropolitan Water District's OC-35 connection. The City's combined capacity is 15.36 mgd (10,500 gpm).

The maximum daily demand, peak hour demand and fire flow requirements of development within the Specific Plan area can be met with the City's existing supply capacity. All water mains are proposed to be dedicated to the City through easements and/or through public dedication for road right-of-way purposes.

3.2 SEWER SYSTEM PLAN



FIGURE 3-1
Sewer Manhole Installation

The provision of adequate sewerage facilities is an important prerequisite to development. The sewerage facilities in Seal Beach are composed of shared, public pipelines and related facilities which collect and transport water-borne wastes away from individual homes or businesses, treat the waste and then dispose of it in a manner that is not detrimental to the public health or to the environment. Collection and

treatment of sewerage in Seal Beach is accomplished by a combination of City and County facilities. This includes pipes, pump stations and treatment plants, all of which have the capacity to serve development of properties within the BIDS Specific Plan area. Local wastewater is collected and conveyed through Seal Beach facilities to the Orange County Sanitation District Plant No. 2 in Huntington Beach where it is treated. The treated effluent is then disposed of through the existing ocean outfall.

The BIDS Specific Plan is located within District 3 of the Orange County Sanitation District. Existing BIDS facilities connect into the City's lift station at Westminster Avenue. New development will include additional sewerage facilities, including service lines, force mains, lift stations and/or modifications to the existing City lift stations (Figure 3-2, Water and Sewer Facilities Plan).

3.3 STORM DRAINAGE PLAN

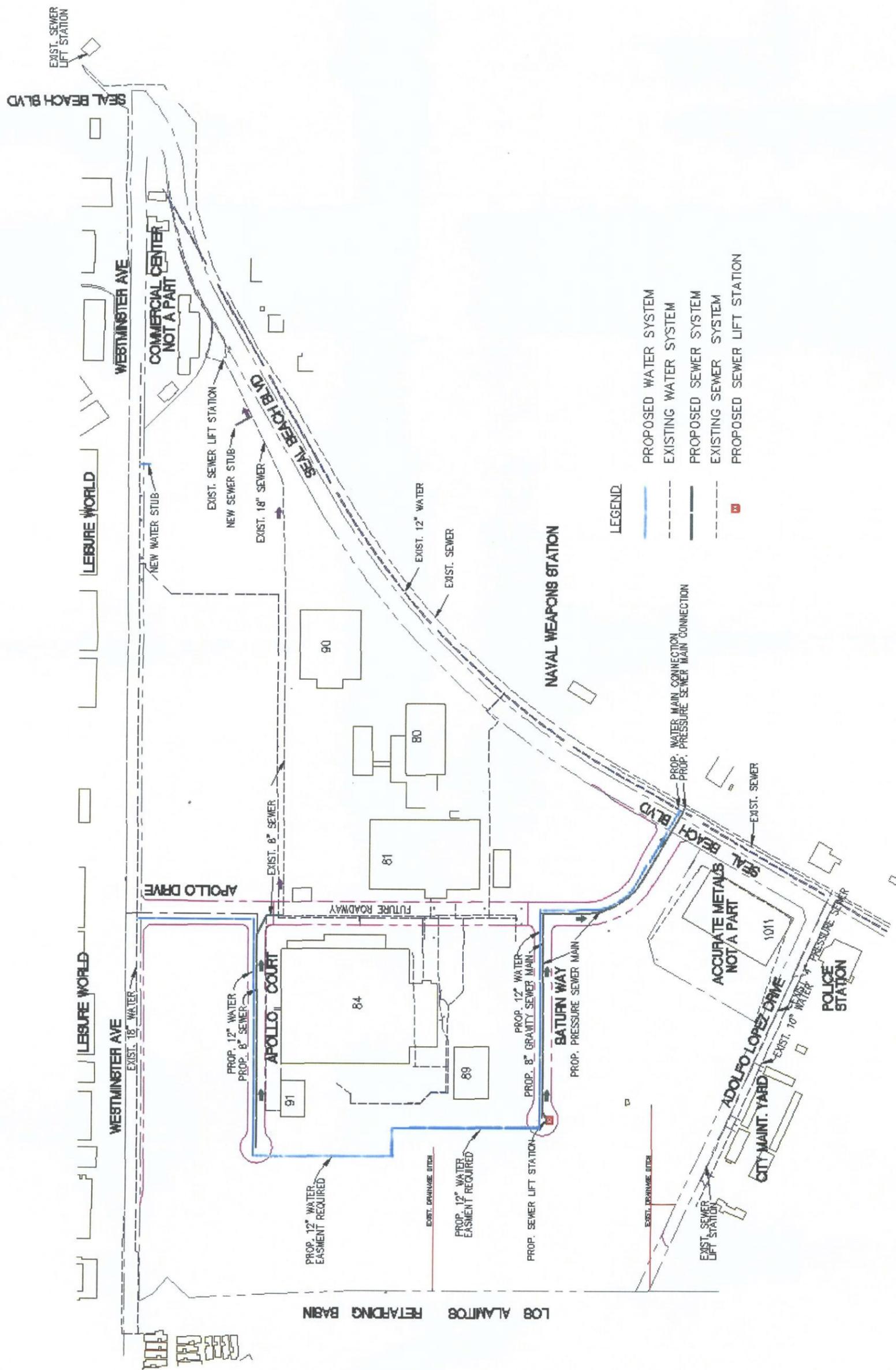
The BIDS Specific Plan area is developed with a series of private catch basins, storm water pipes and man-made open drainage ditches that convey runoff from the undeveloped and developed portions of the site to the adjacent Los Alamitos Retention Basin (LARB). The LARB is a 35-acre regional flood control facility owned and operated by the Orange County Flood Control District (OCFCD). The LARB provides for collection and storage of storm water runoff from a 5,000-acre watershed, prior to being pumped into the San Gabriel River.

The BIDS Specific Plan project includes use of, and extensions to, existing facilities to continue to control storm water on the site as well as construction of new facilities (Figure 3-3, Storm Drain and Water Quality Concept Plan). The ultimate location and sizes of the proposed storm drain lines may vary from that shown on the plan.

3.4 WATER QUALITY

Water quality in California is primarily regulated by the National Pollution Discharge Elimination System (NPDES), under the federal Clean Water Act. The State of California Water Resources Control Board (SWRCB) and the Regional Water Quality Control Board (RWCQB) administer both State of California and NPDES regulations.

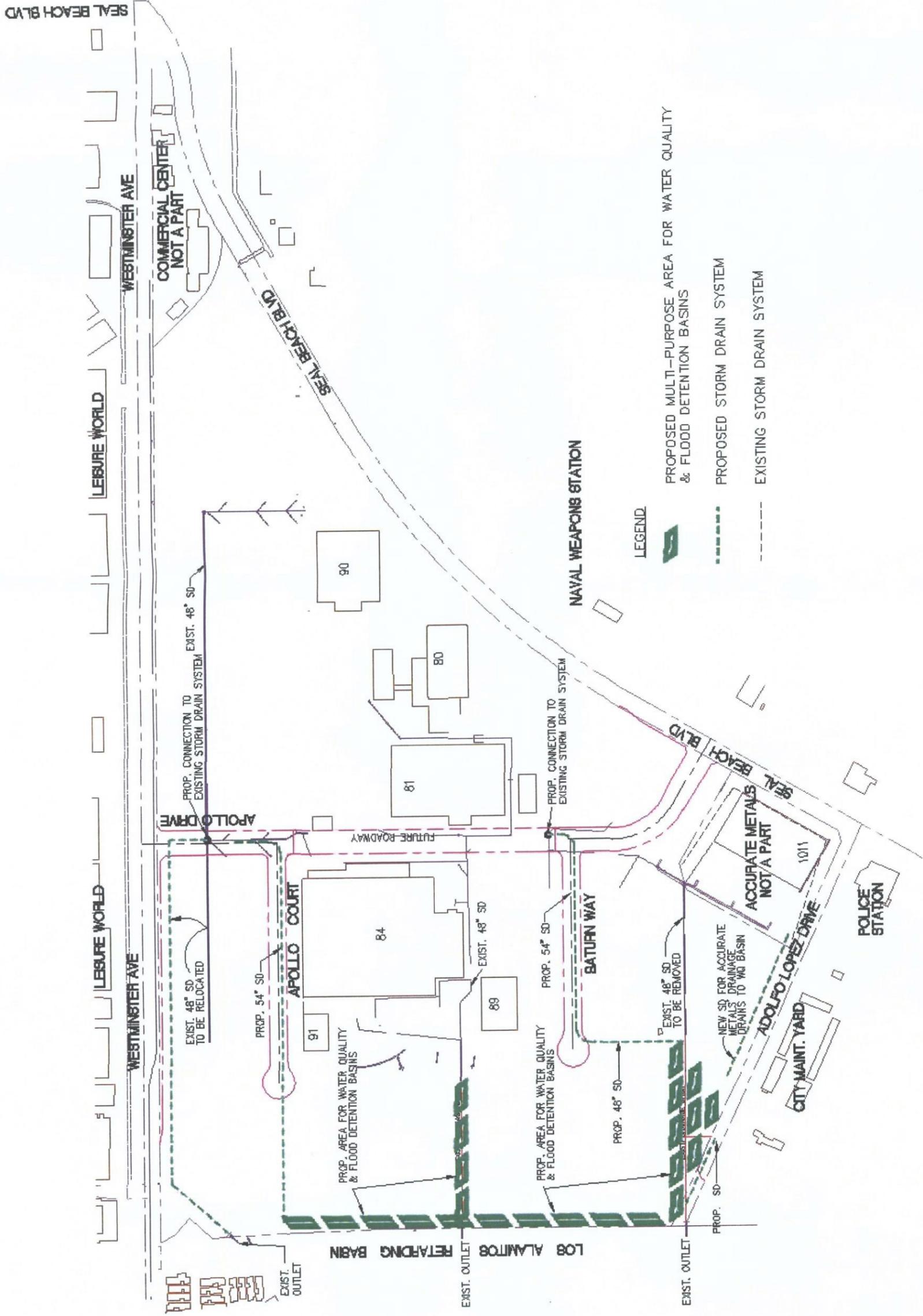
Locally, the County of Orange has developed a Drainage Area Master Plan (DAMP) and the City of Seal Beach has adopted a Storm Water Quality Management Plan (SWQMP). The SWQMP requires implementation of, and compliance with, all local, state and federal regulations regarding water quality. The BIDS Specific Plan provides for provisions to be consistent with all local, state and federal regulations.



LEGEND

	PROPOSED WATER SYSTEM
	EXISTING WATER SYSTEM
	PROPOSED SEWER SYSTEM
	EXISTING SEWER SYSTEM
	PROPOSED SEWER LIFT STATION

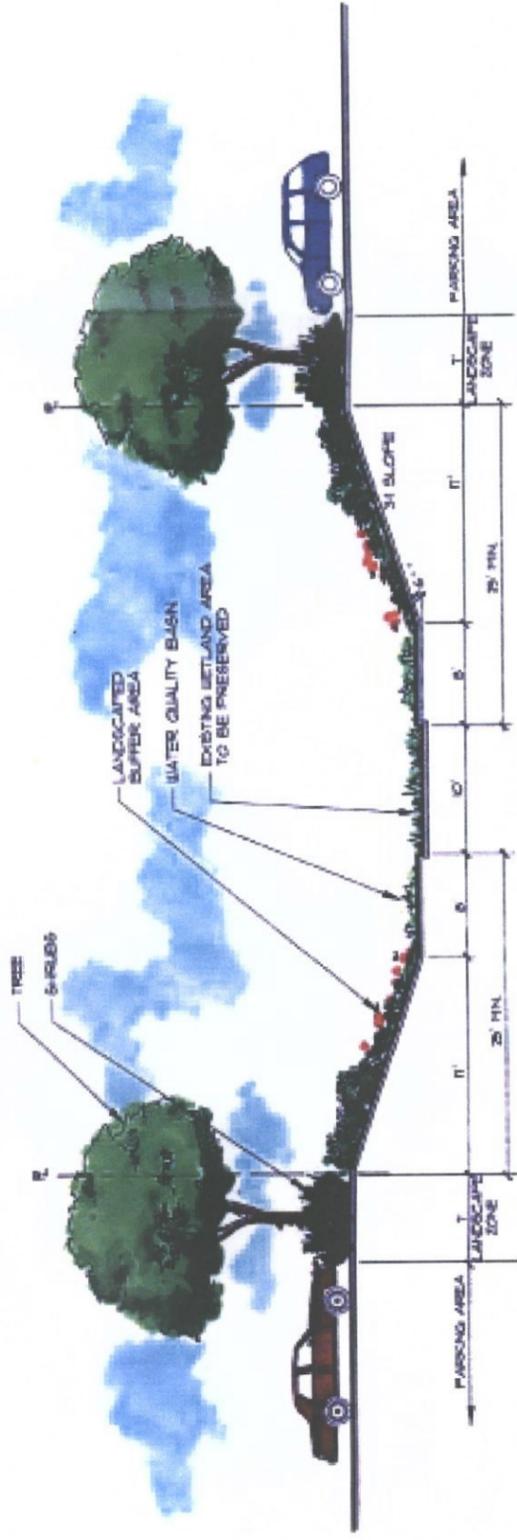
FIGURE 3-2
CONCEPTUAL WATER AND SEWER FACILITIES PLAN



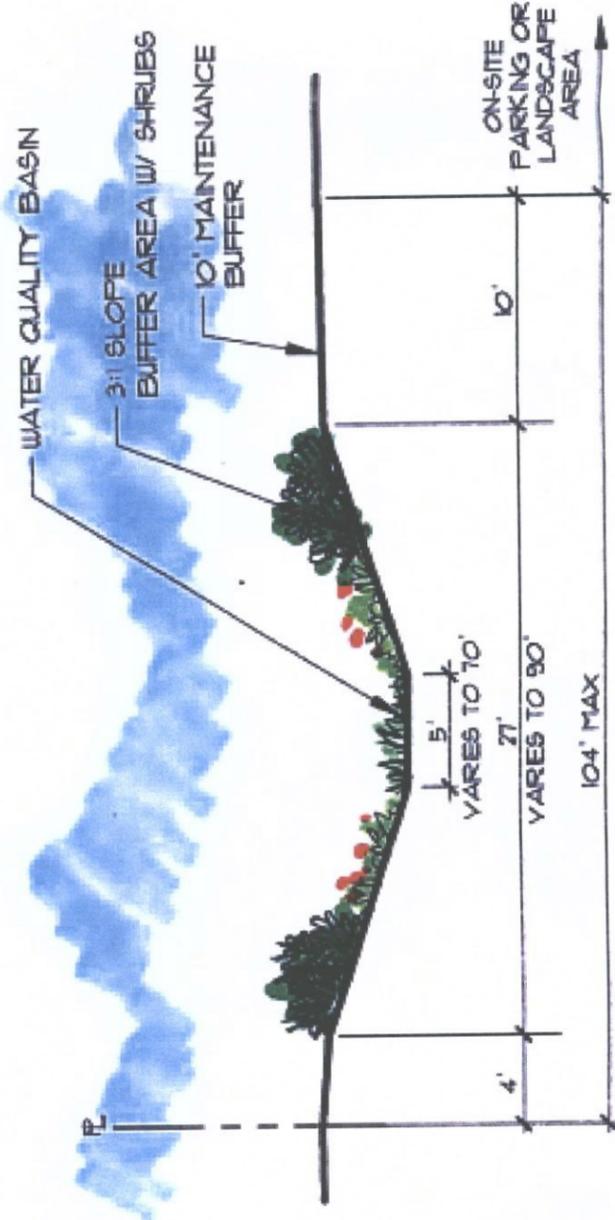
LEGEND

-  PROPOSED MULTI-PURPOSE AREA FOR WATER QUALITY & FLOOD DETENTION BASINS
-  PROPOSED STORM DRAIN SYSTEM
-  EXISTING STORM DRAIN SYSTEM

FIGURE 3-3
STORM DRAIN AND WATER QUALITY CONCEPT PLAN



Section I-I' - Center Drainage Conveyance/ Retention Area



Section J-J' - Water Quality Basin

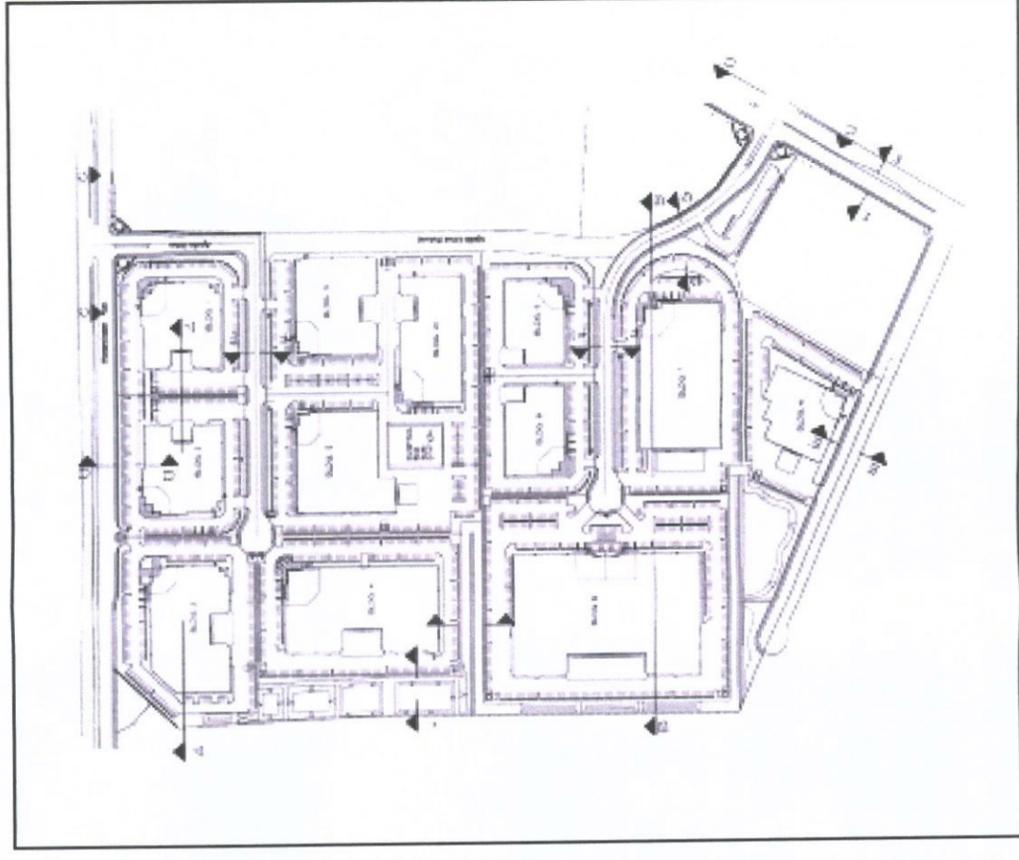


FIGURE 3-4
WATER QUALITY BASIN AND RETENTION CONCEPT

The water quality management concept for the BIDS Specific Plan is to:

- (a) Implement a variety of structural and non-structural Best Management Practices (BMPs); and,
- (b) Establish certain maintenance procedures and other management practices to prevent and/or reduce the pollution of downstream receiving facilities (i.e. the Los Alamitos Retarding Basin).

Typical elements of such a BMP program will include the use of City-approved catch basin filters and may also include the use of oil and grease traps, bio-filtration, local detention basins, vegetation filter strips and inlet separation devices. The intention is to use, expand and enhance the central and southern drainage ditches for storm water runoff and water quality purposes (Figure 3-3, Storm Drain and Water Quality Concept Plan).

A final Water Quality Management Plan for the Specific Plan area will be prepared prior to issuance of any grading permit.

Other public services and utility providers that service the BIDS Specific Plan area are identified below.

3.5 SOLID WASTE

Solid waste generated by the BIDS Specific Plan will be disposed of at county landfills. Within the City of Seal Beach, solid waste is currently picked up and recycled and/or disposed of by a private company selected by the City. Based on service projections and anticipated demand increase, an adequate level of service will be maintained for the Specific Plan area. No solid waste disposal facilities are planned to be located in the Specific Plan area.

3.6 ELECTRICITY

The BIDS Specific Plan area is currently located within the service area of the Southern California Edison (SCE) Company. Existing transmission and distribution lines are adequate to service current and potential future needs. Individual development projects may require relocating existing facilities, including the on-site sub-station, concurrent with other improvements. All new services will be provided underground.

3.7 NATURAL GAS

Natural gas service in the Specific Plan area is currently provided by the Southern California Gas Company. Development projects will require extensions and possible relocations of natural gas facilities.

3.8 TELEPHONE

Telephone service in the Specific Plan area is currently provided by General Telephone (GTE). Coordination with GTE will be required for relocation of existing facilities and installation of new service. All new services will be provided underground.

3.9 CABLE TELEVISION

Cable television service within Seal Beach is currently provided by Adelphia Communications. Coordination with this company will be required for the installation of new service. All new services will be provided underground.

3.10 POLICE SERVICES

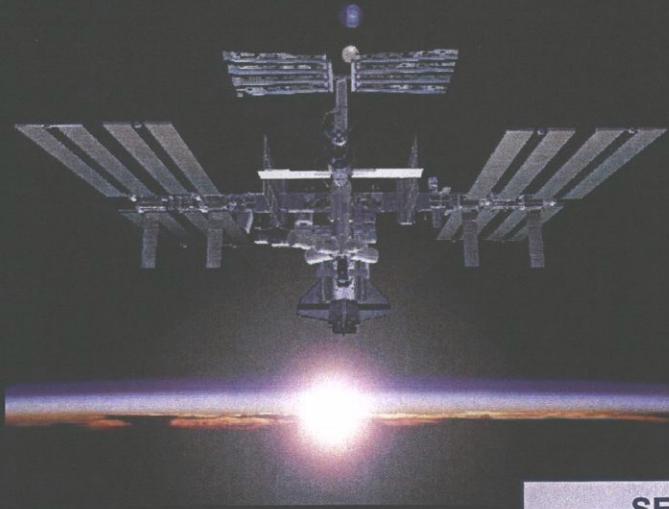
The City of Seal Beach Police Department currently provides police services to the BIDS Specific Plan area. The Department currently has thirty-seven police officers, sixteen civilian employees, eight reserve police officers, and forty senior volunteers providing quality service to the City of Seal Beach.

3.11 FIRE AND EMERGENCY SERVICES

The Orange County Fire Authority (OCFA) currently provides fire prevention/suppression and emergency services to the City of Seal Beach. Stations serving the City of Seal Beach include Station 44 at 718 Central Avenue and Station 48 at 3131 Beverly Manor Road.

Boeing Realty Corporation





**SECTION FOUR
DESIGN GUIDELINES**

BOEING
INTEGRATED DEFENSE SYSTEMS
SPECIFIC PLAN
SEAL BEACH SITE

Boeing Realty Corporation



SECTION FOUR

DESIGN GUIDELINES

4.1 INTRODUCTION AND THEME

The BIDS Specific Plan is a planned community for existing and planned light industrial business, research and development, office and commercial uses.



**FIGURE 4-1
BUILDING ARTICULATION
AND FENESTRATION**



The design guidelines in this document define the general criteria for implementing coordinated design, organizational unity and overall visual identity for the new areas to be developed, while maintaining opportunities for specific needs and

creativity for each project. Included are parameters for integrated site planning, architecture, landscaping and exterior lighting, as well as procedures and requirements

for design submittal and review. The intent of these guidelines is to establish a consistent design concept that produces a clear image and a sense of prestige, efficiency and integrity.

The BIDS Specific Plan has an overall coordinated design character that emphasizes a clean, contemporary, straightforward and quality image. This image is expressed in site planning, architecture, landscaping, lighting, and signage. Architectural design is to be compatible in character, massing and materials throughout the BIDS Specific Plan area, while allowing for individual identity and creativity in each project.

The design guidelines have been developed to be "guidelines" as opposed to "development regulations" identified in Section 5, and should not be interpreted to require stringent compliance with any particular element. To promote the quality of design planned for this project, the design guidelines given in this document establish criteria that enhance the coordination, organization, function and identity of the site, while maintaining a compatible relationship with the surrounding development of the BIDS Specific Plan.

4.2 SITE PLANNING

Site planning guidelines for the BIDS Specific Plan have been developed to create visual consistency and promote the image of a quality business park. Site planning is to be developed in a manner that emphasizes a clean, pleasant and contemporary environment. The goals of the site planning guidelines are to control building placement, enhance entry effects, organize vehicular and pedestrian circulation, address future expansion for buildings and parking, and lessen the influence of disruptive elements such as service areas.

To facilitate the development of the BIDS Specific Plan into a unique



FIGURE 4-2
ARCHITECTURAL CONCEPT
Building Entry

“The design guidelines define the general criteria for implementing coordinated design, organizational unity and overall visual identity for the area, while maintaining opportunities for specific needs and creativity for each project”

resource for the community, the following site planning policies will be utilized by the City in reviewing Precise Plan applications for

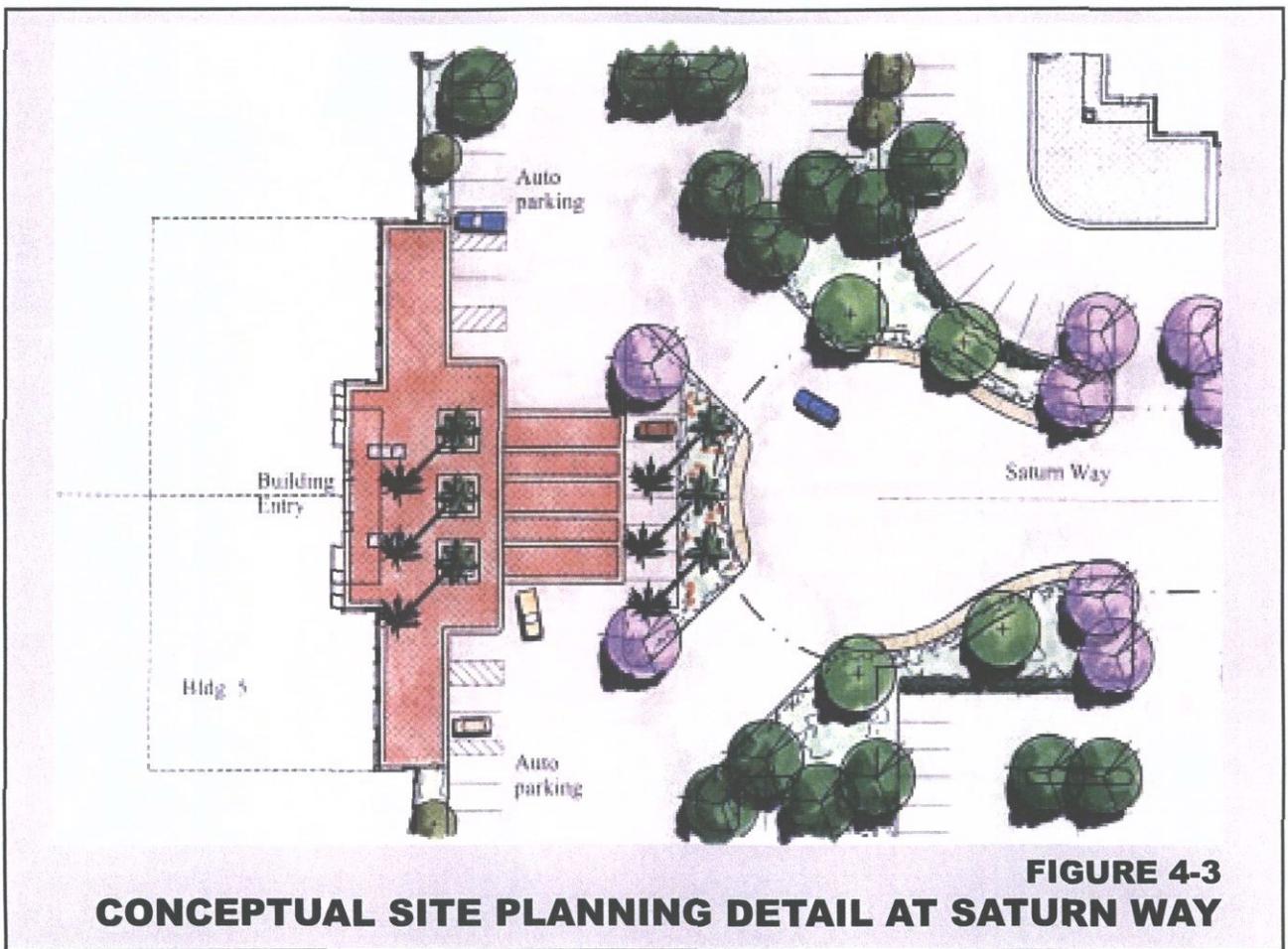
development within the BIDS Specific Plan area.

4.2.1 Building Locations

Buildings are to be located on each site in a manner that is efficient, appropriate to site conditions, consistent with the overall architectural composition and

compatible with nearby projects and development throughout.

- Buildings should be located to enhance project visibility and identity, while maintaining compatible relationships with adjacent properties and street frontages.
- Buildings should enhance the characteristics of the site.
- Buildings should take advantage of local climate and achieve optimum energy efficiency.
- Buildings should be oriented so that loading and service areas are not visible from the street, and are screened from public view and residential



areas to the extent feasible. Additional screen walls and landscaping should be considered to reduce potential visual and noise impacts.

- Buildings should be arranged to provide convenient access to entrances and efficient on-site circulation for vehicles and pedestrians.
- Multiple buildings on a single site should be oriented to promote efficient auto and pedestrian circulation. Buildings should also be arranged to permit development of usable outdoor spaces.

4.2.2 Parking

- Parking areas are to be landscaped to provide a pleasant appearance.
- Any parking beneath buildings or in parking structures shall be screened by architectural design or landscaping.
- Parking areas for motorcycles and bicycles are to be designed for orderly, uncluttered parking. Bicycle parking areas are to be provided with racks and locking capabilities.

- Designated spaces shall be provided for handicap, carpool, motorcycle and bicycle parking, as required by the State of California and the City of Seal Beach.

4.2.3 Pedestrian Circulation

Safe, clear pedestrian circulation shall be provided between buildings, parking areas and entries. Americans with Disabilities Act (ADA) compliant pedestrian circulation shall be provided from parking areas to building entries and to streets.

4.2.4 Service Areas

Service, storage, maintenance, loading and refuse collection areas should be located out of view of residential areas, public roadways and buildings on adjacent sites, or screened by dense landscaping and/or architectural barriers to the extent feasible. Service areas should be located so that service vehicles have clear and convenient access that does not disrupt vehicular and pedestrian circulation or create "blind spots" for vehicular traffic. Loading and unloading is prohibited on public streets.

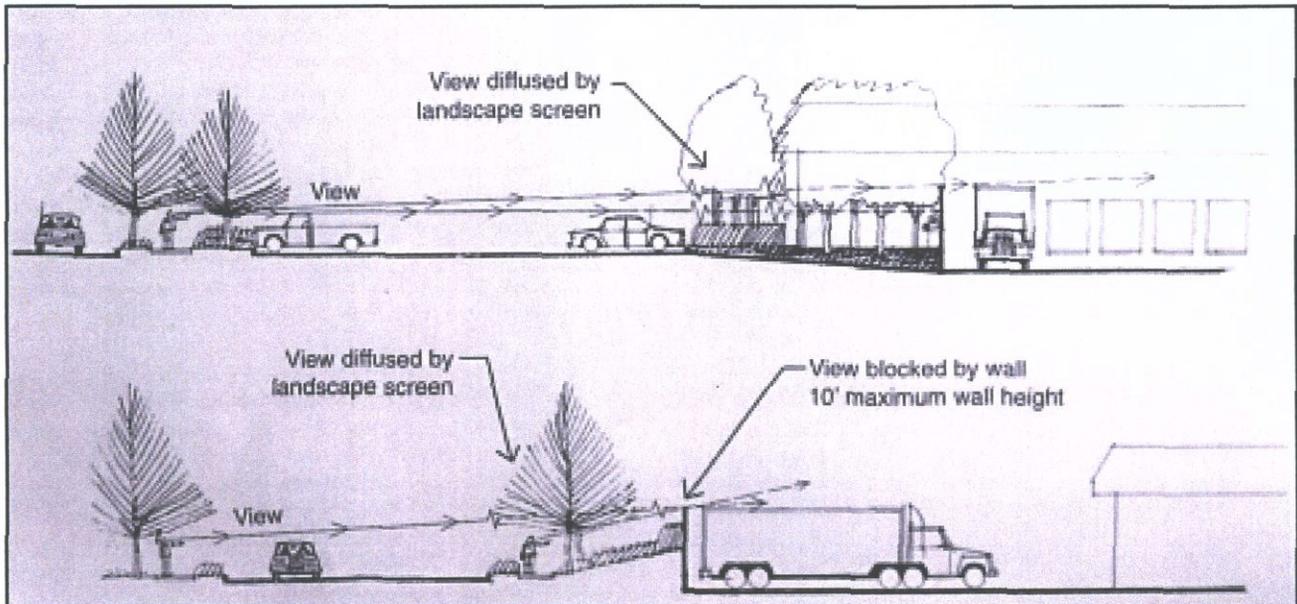


FIGURE 4-4

CONCEPTUAL SCREENING OF LOADING AND STORAGE AREAS

4.2.5 Grading and Drainage

- Grading and drainage shall be designed so that surface drainage and soil erosion does not flow off the BIDS Specific Plan area.
- Treatment areas for water quality purposes, including BMPs, shall be incorporated into final plans.
- Drainage shall be directed away from building entrances and pedestrian walks. On-site grading shall be designed to be compatible with streetscape grades.

- The grading concept shall be designed to avoid ramps from the public sidewalk to the site and from the parking lot to the building.
- Concrete swales in parking lots shall not be located in the center of the drive aisles, but should be located at the edge of the parking spaces and /or curb.

4.2.6 Utilities

- New building utility systems, including water, electricity, gas, sewer and storm drains shall be installed underground. Necessary above ground equipment such as back-flow preventers, transformers and irrigation controllers should be located away from the street frontage-landscaping zone. When possible, locate all meters and appurtenances in one general location.
- Utilities should be located in a common trench where possible and in accessible locations. Incoming utilities should be sized for possible expansion when appropriate.
- To provide visibility at intersections, pedestrian safety, and a high quality appearance, any necessary above ground equipment (such as double detector check assemblies, backflow preventers for the domestic water meter, and transformers) should be located behind the streetscape hedge and screened, if possible.

4.2.7 Walls and Fencing

Walls, when used, should be designed as an integral part of the overall architectural or landscaping design concept. No walls are permitted along streets within landscaping setback areas, except for security purposes. Refuse enclosures should be equipped with self-closing gates. All walls should be built of materials and finishes that are compatible with adjacent buildings.

Fences are not permitted in the areas to be developed except for security purposes, screening and adjacent to the LARB and water quality treatment areas.

4.2.8 People Places

Forecourts to buildings should provide an introduction, a drop-off area, and a pause before guests enter buildings. The design should discourage the use of this area for lunch and coffee breaks. People gathering areas should be oriented such that they are protected from the prevailing winds and in the hot season sheltered from sun exposure. Trees, walls and overhead structures may be used for climate mitigation. Site furnishings are encouraged to provide intuitive destinations for people to sit and participate in the gathering areas.

4.3 ARCHITECTURAL

The objective of the architectural design guidelines is to establish a consistent aesthetic theme throughout the development of the business park. Each project, however, should be responsive to the context in which it is located. All considerations should be made relevant to climate and weather conditions, as well as the existing, natural and architectural context. Individual creativity and identity are encouraged, but care must be taken to maintain design integrity and compatibility among all projects in order to establish a clear, unified image for the BIDS Specific Plan area.



FIGURE 4-5
ARCHITECTURAL CONCEPT
Contemporary Architectural Style

The following guidelines present parameters for architectural character, building form and massing, façades, fenestration, structure, entries, materials, colors, details, service areas and mechanical equipment for the business park.

4.3.1 Architectural Character

The architectural character should be determined as the result of both the natural and architectural context

of each specific site. While the architecture should respect local and regional influences, the overall result should still remain contemporary in nature. An acceptable character should remain clean and simple in a manner that is both progressive and timeless.

Appropriate Treatment:

- Contemporary, classic, technical style
- Clean, smooth, efficient lines and forms
- Distinctive, but compatible image

Inappropriate Treatment:

- Trendy or historical styles
- Complicated, arbitrary forms
- Sharp contrast with surrounding environment
- Non-responsive to region or climate

4.3.2 Building Form and Massing

The design should incorporate clean, simple, geometric forms. Whenever possible, function should be identified through the form of the structure. Similar forms should identify similar functions throughout the project. A definite sense of human scale should be incorporated through sensitive massing and forms. Compositions



FIGURE 4-6
ARCHITECTURAL CONCEPT
Clean, Simple, Geometric Forms

should be coordinated to produce an overall sense of unity throughout each new project. Massing should always respect both the natural and architectural context of the site. Care should be taken to transition larger forms and masses from pedestrian areas.

Appropriate Treatment:

- Straightforward geometry
- Unified composition
- Expression of structure
- Expression of function and programmatic elements through form
- Delineation of human scale

Inappropriate Treatment:

- Complicated forms
- Arbitrary, inconsistent composition
- Monolithic blocks

4.3.3 Façades/ Fenestration

Building façades should reflect a coordinated design concept. Reinforcing the massing and form of each project, the façades should express building function, structure and scale. Fenestration should strive to delineate building uses while remaining sensitive to existing architectural context. Fenestration should be designed to optimize energy efficiency.



FIGURE 4-7
ARCHITECTURAL CONCEPT
Building Materials Variety

Appropriate Treatment:

- Straightforward, maintainable, functional design
- Expression of structure
- Unity and scale reinforced through an integrated grid module
- Surface delineation through expression of reveals, mullions, recesses, and structure
- Appropriate balance of wall and glazed surfaces
- Functional and energy conscious use of glass and glass patterns
- Shading devices and elements

Inappropriate Treatment:

- Arbitrary, inconsistent forms and decoration
- Inappropriate decorative glass patterns
- Overly exaggerated setbacks or reveals between materials

4.3.4 Entrances

Building entrances should be clearly defined and easily recognizable. Whenever possible, entrances should be enhanced through massing and fenestration. The entry should always be an integral part of the overall composition of the building. Overhead protection as well as security should always be addressed. Entries may provide opportunities for signage, color, and accent lighting.

Appropriate Treatment:

- Articulation and color for identity and recognition
- Light, open and inviting
- Recessed, protected doorways
- Integration with overall building form, site organization and entry sequence
- Coordinated landscaping
- Sensitive to regional and climatic conditions

Inappropriate Treatment:

- Exaggerated, overscaled forms and color
- Dark, confined, or hidden entries
- Abrupt entry sequences

4.3.5 Exterior Materials

Building materials should enhance the overall form and massing of an individual structure. Materials should be appropriate for local climate conditions, easy to maintain, and appropriate for the function of the facility. The use of enriched materials should be encouraged in areas where they will provide the most visual impact. Care should be taken to choose materials that are sensitive to sustainable building objectives.

Appropriate Materials:

- Materials which enhance building form
- Easily maintained highly durable materials
- Sustainable materials
- Clean, finished, and contemporary materials
- Materials which are appropriate to function

Inappropriate Materials:

- Highly industrial materials, unless appropriate to function
- False or simulated materials
- Materials not suited for the environment in which they are being used
- Highly reflective materials
- Experimental, untested, unproven materials

4.3.6 Exterior Colors

Colors should be used to enhance forms and masses. Whenever possible, color should be inherent to the materials. Natural stones, concrete, and metal are encouraged to be used whenever possible. Color should always be used as an integrated design feature.

Appropriate Treatment:

- Colors which are integral to the material
- Colors which enhance design features and building massing
- Colors which express the regional nature of the facility
- Timeless, classic color palettes

Inappropriate Treatments:

- Garish use of color
- Arbitrary patterns or stripes
- Trendy colors
- Primary colors

4.3.7 Mechanical Equipment

All exterior mechanical equipment, including HVAC, electrical equipment, storage tanks and satellite dishes, should be screened from adjacent public streets.

Whenever possible, mechanical screening elements should be integrated into the architectural composition.

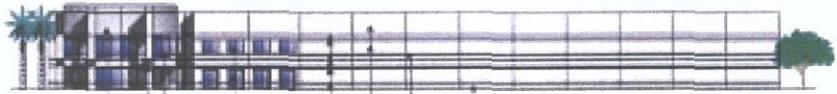


FIGURE 4-8
ARCHITECTURAL CONCEPT
Technical Style

Appropriate Treatment:

- Rooftop equipment screened from off-site view by building parapet or mechanical screens
- Mechanical screen fully integrated into architectural composition
- Ground equipment screened by walls or landscaping, or covered in multi-story projects
- Screen walls of same or similar building materials as adjacent building walls

Inappropriate Treatment:

- Isolated rooftop screen "hat" which is not integrated into the building form or mass
- Exposed rooftop equipment
- Contrasting screen material

4.4 LANDSCAPE DESIGN GUIDELINES

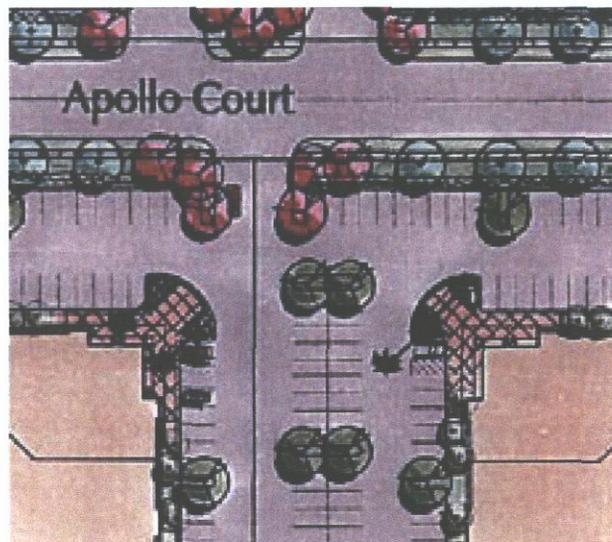
The primary objective of the Landscape Design Guidelines is to promote a pleasing, responsive, and unified environment within the project area. All landscaping is designed to:

- Promote a pleasant, distinctive, environment.
- Augment internal cohesion and continuity.
- Enhance the structured design concept of the project area.

- Promote water conservation.

Thus, the landscaping design concept is focused toward:

- Providing a clean, contemporary visual appearance.
- Coordinating the landscaping treatment along arterial and surface streets within the circulation system.
- Coordinating streetscapes to unify its general appearance.
- Coordinating on-site landscaping design continuity among individual development sites.



**FIGURE 4-9
COORDINATED
LANDSCAPE APPEARANCE**

The following guidelines present parameters for general landscape design, water conservation, and on-site landscaping.

4.4.1 Irrigation and Water Conservation Measure

All landscape areas are to be provided with a complete, automatic irrigation system. This system shall conform to all requirements of the City of Seal Beach. Landscape design should consider the following conservation measures:

- Site analysis of macro and microclimates, solar exposure, prevailing wind conditions, seasonal temperature patterns, soils and drainage, and grades and slopes.
- Use of historical evapo-transpiration rates and weather station (CIMIS) data.
- Use of planting zones coordinated according to plant type, climatic exposure, soil condition and slope to facilitate use of zoned irrigation systems.
- Use of low water or California natural plant species.
- Landscapes with “climate adaptive” schemes that once established can be weaned from irrigation systems or use minimal supplemental water. Invasive vegetation shall not be permitted.

Irrigation system design should consider the following water conservation measures:

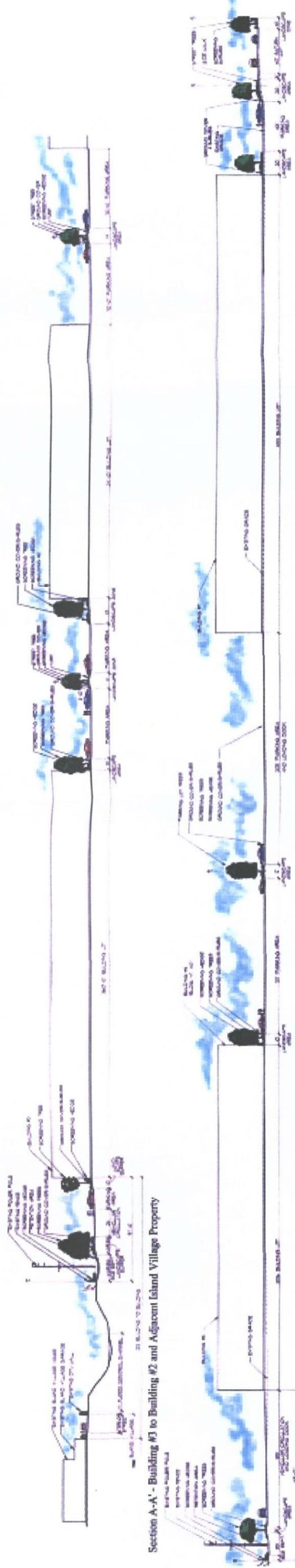
- Audit of water use and certification by a licensed landscape architect of the irrigation system.
- Use of “state of the art” irrigation technology to maximize efficient use of water, including multi-program electronic timers, rain shut-off devices, remote control valves, drip systems, pressure reducing valves, and matched output sprinkler heads.
- Use of gate valves to isolate and shut down mainline breaks.
- Designs to deeply soak the root growth zones while avoiding flow rates that exceed infiltration rate of soil.
- Designs to prevent over-spray or discharge onto roadways, non-landscaped areas or adjacent properties and building surfaces.
- Timing of irrigation cycles to be set at night when wind, evaporation and people activities are at a minimum.
- Use of reclaimed water systems if practical and available.

4.4.2 On-Site Landscaping

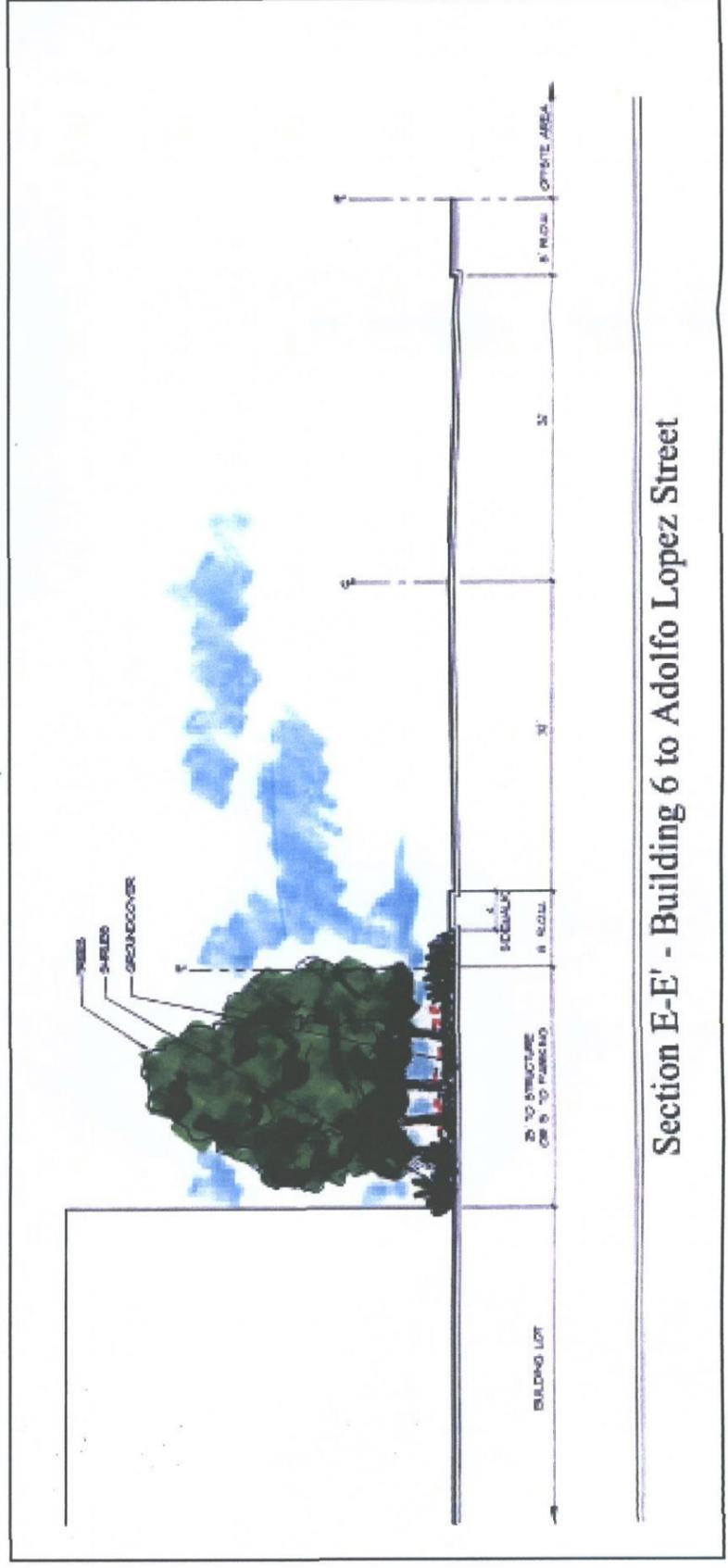
On-site landscaping is to be coordinated in a manner that enhances overall continuity of development in the BIDS Specific Plan, while providing for the individual identity and needs of each project within. On-site landscaping shall be consistent and compatible with that for the street-side landscaping setbacks. In addition, on-site landscaping shall enhance the building environment, screen parking and service areas, and provide protection from exposure to the sun and wind. The landscape architect, when preparing concepts and designs, should consider the following:

Functional Considerations:

- Solar orientation of exterior areas.
- Views.
- Separation of functional uses and creation of exterior spaces such as courtyards for employees.
- Reinforcement of the vehicular and pedestrian circulation system with plantings.
- Climatic mitigation of pedestrian spaces and corridors.
- Shelters from traffic noise and hazards.



Elevation C-C' - Westminster and Seal Beach Boulevard Entries



Section E-E' - Building 6 to Adolfo Lopez Street

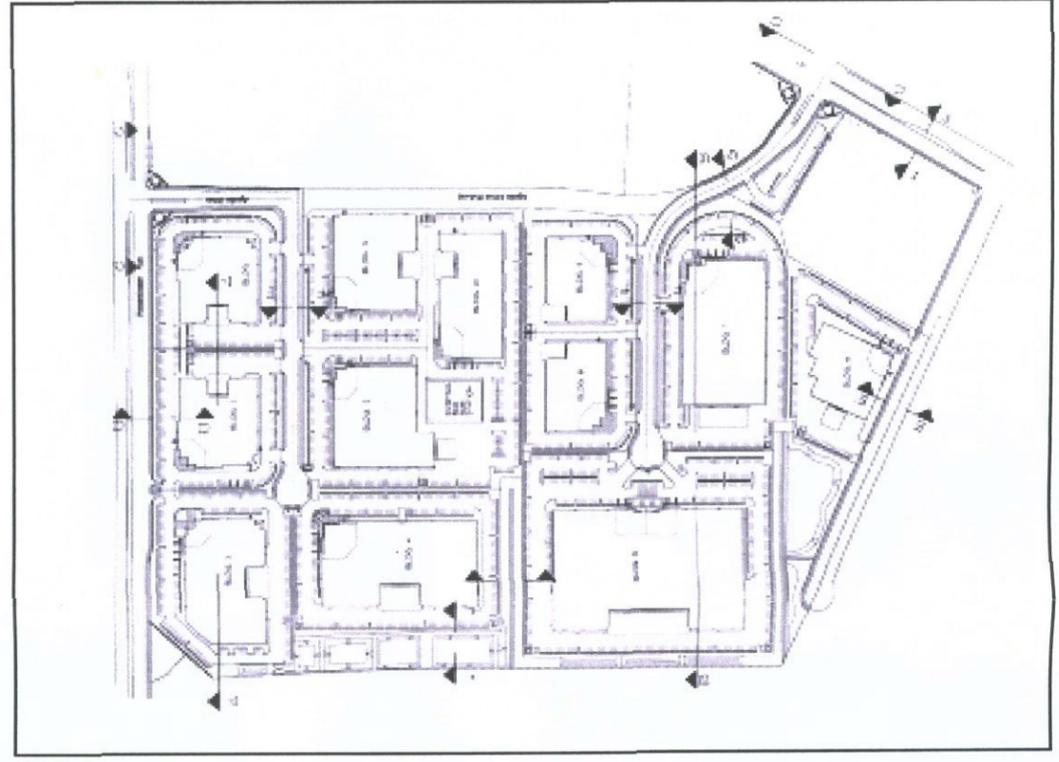
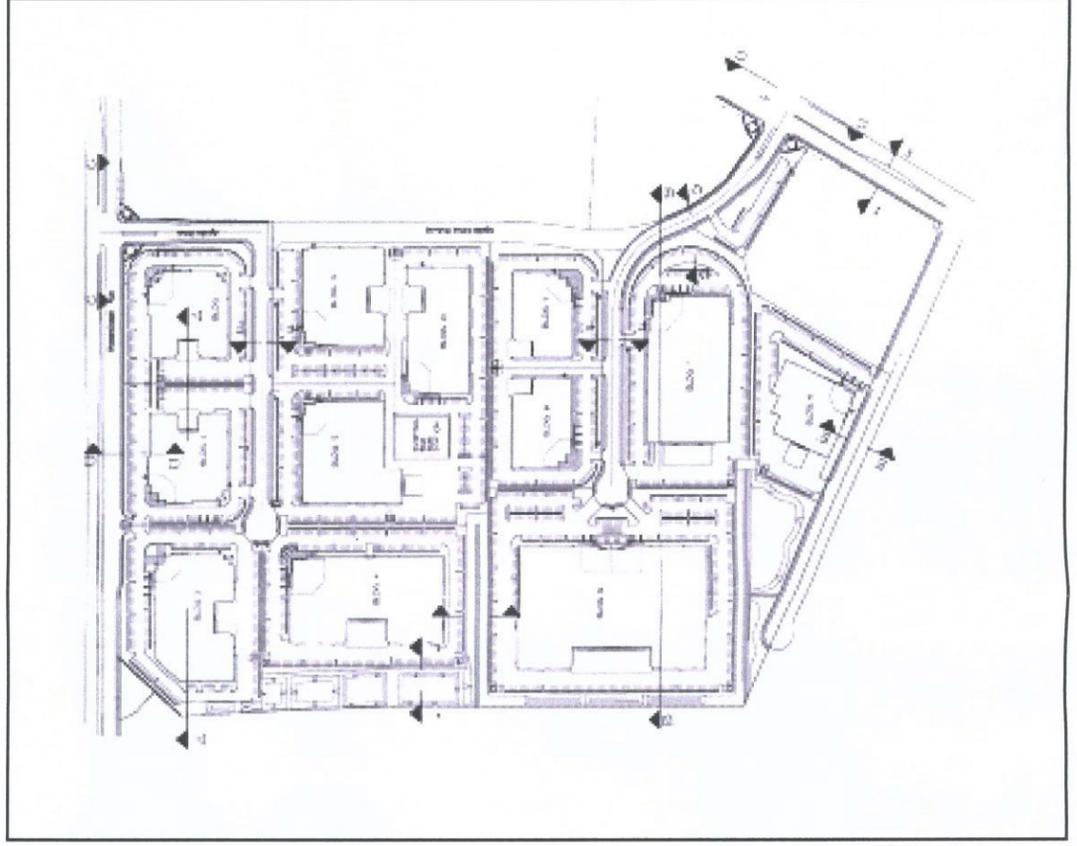
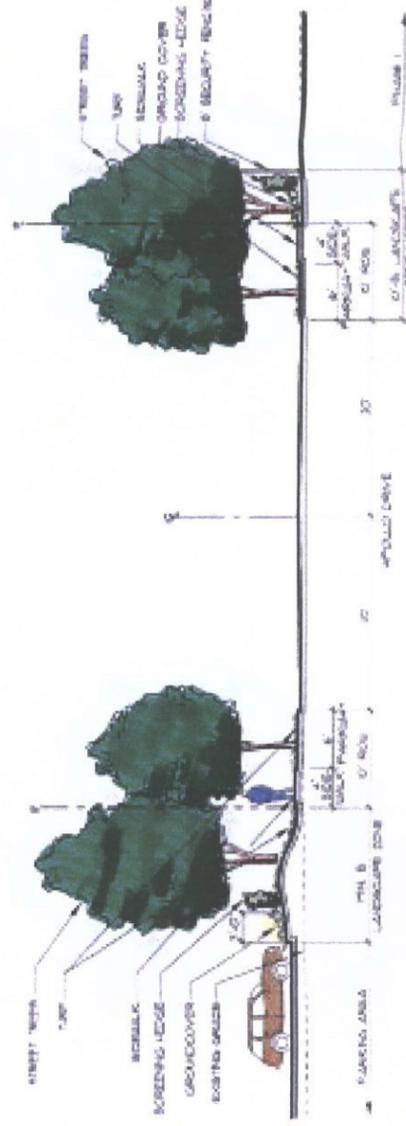
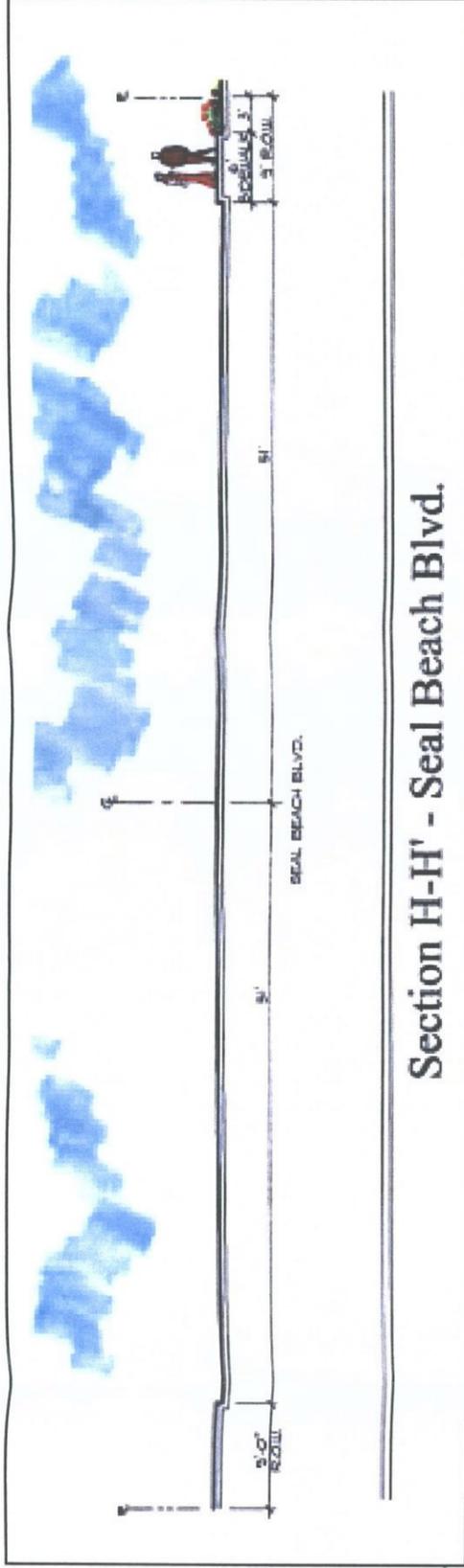
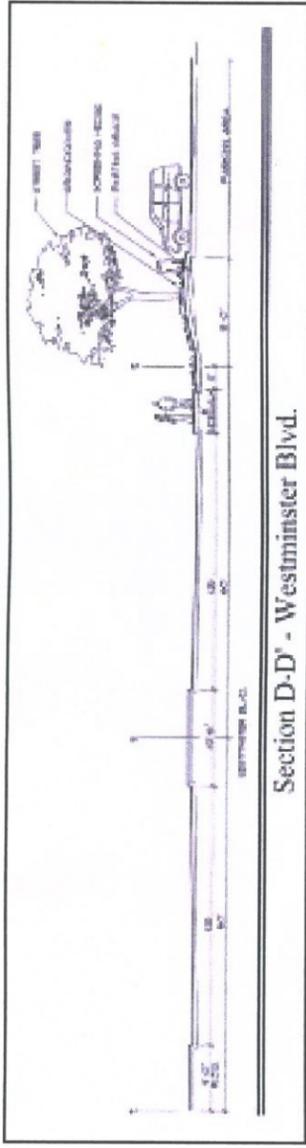


FIGURE 4-10
CONCEPTUAL LANDSCAPE SECTIONS



- Plant selection to maximize long-term ease of maintenance and optimize water conservation.
- Plant material selections that are appropriate to the planting spaces available and the proximity to the constructed elements such as buildings, paving and walls.
- Compatibility with size and type of existing planting on site or adjacent to the site.
- Integration with building design.
- Accommodation of future expansion.
- Use of native plant material to the extent practicable.

Aesthetic Considerations:

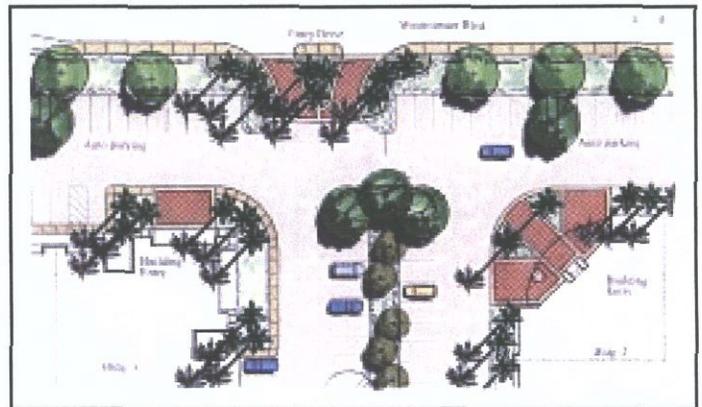
- Design compatibility with the perimeter landscape concepts.
- Creation of a central landscape focus in relation to the heart of the development (building entry, central courtyard, etc.).
- Special lighting in public open spaces.
- Use of plants and design elements for color in the landscape.

4.4.3 Project Entry

The entry zone is the area between the street and the main building entry. It generally includes the on-site entry road, parking access points, visitor parking, and building entry area. The entry zone should be treated with special landscape elements, which will give individual identity to each project.

The objectives of the landscape design for entry areas will be to:

- Clarify the visitor path to the front door.
- Establish a unique image for the development.
- Establish a sense and sequence of entry from the public street to the more private realm.



**FIGURE 4-12
SHARED ENTRY CONCEPT**

4.4.4 Parking Areas

The parking areas include parking spaces, drive aisles, parking aisles and other associated vehicular areas on-site. The objectives of the landscape design for parking areas will be to:

- Communicate flow of circulation.

- Visually reduce large paved areas with landscaping.
- Maximize distribution of landscaping.
- Create tree groves in orderly patterns establishing and organizing “parking rooms.”
- Integrate site lighting with the parking aisles and landscape.
- Use minimum size trees and quantities for parking lots of one (1) twenty four (24”) box tree per ten (10) parking stalls. Additional trees may be planted in between stalls.

4.4.5 Building Perimeter

Building perimeter landscaping will provide opportunities for a variety of design approaches, depending on parcel location and building siting.

Landscape design objectives will be to:

- Enhance the building architecture at ground level.
- Allow for landscape expression at the building edge.

Where landscape setbacks are required, landscape areas shall be provided along the building face.

4.5 SIGNAGE AND GRAPHICS

The Signage and Graphic Design Guidelines identify a framework in which advertising a place of business, providing directions or information can be accomplished without detracting from the overall design quality of the project area. The Signage and Graphic Design Guidelines also contribute to the overall project area design theme. Design, color, materials and placement are all important in creating signs that are architecturally attractive and integrated into the overall project area design. The intent is to create and promote a quality visual environment by allowing only signs which are compatible with their surroundings and which effectively communicate their message.

This section sets forth general criteria for new signs within the BIDS Specific Plan area. New signs shall be designed to be architecturally compatible with the colors and materials of the adjacent building. A sign plan, consistent with these guidelines, shall be submitted in accordance with the procedures concurrent with Precise Plan

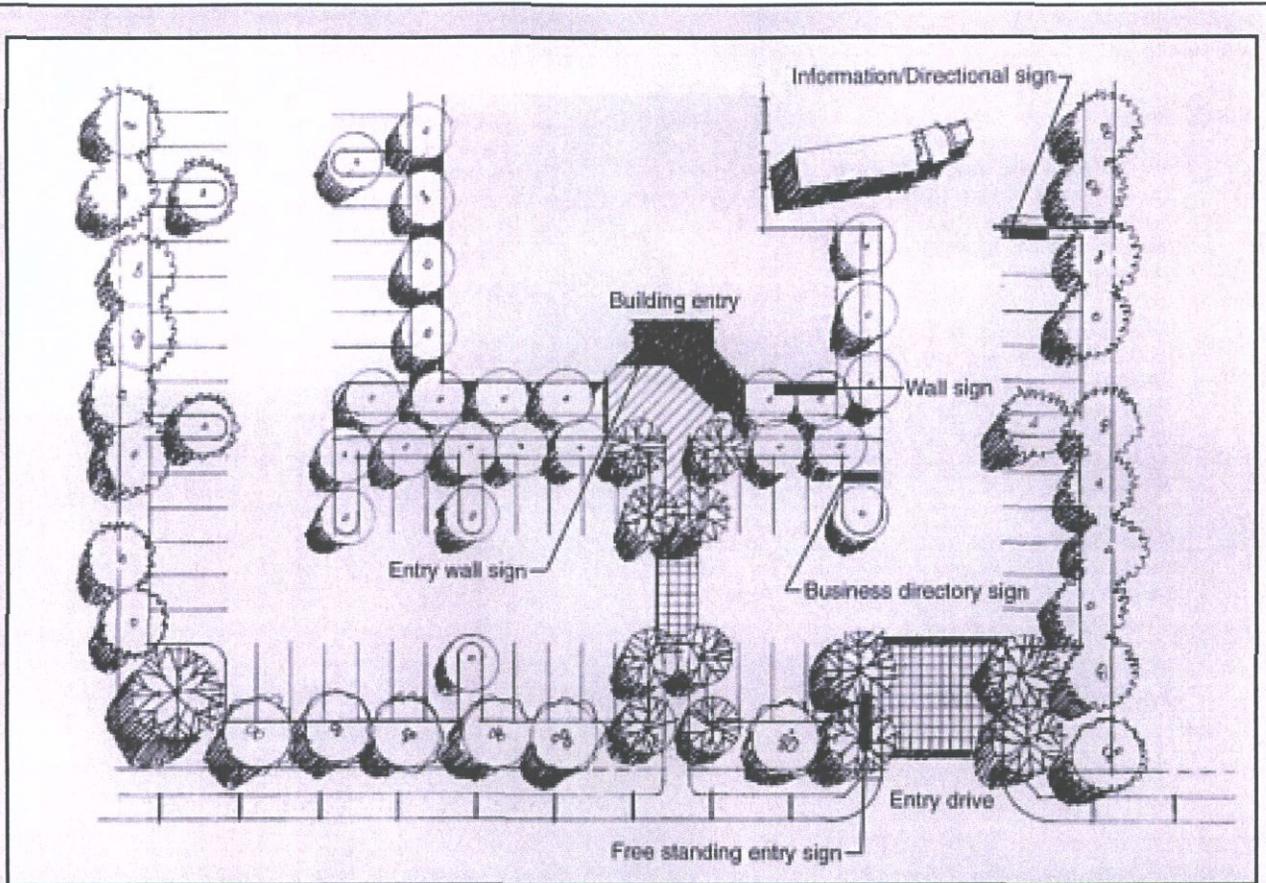
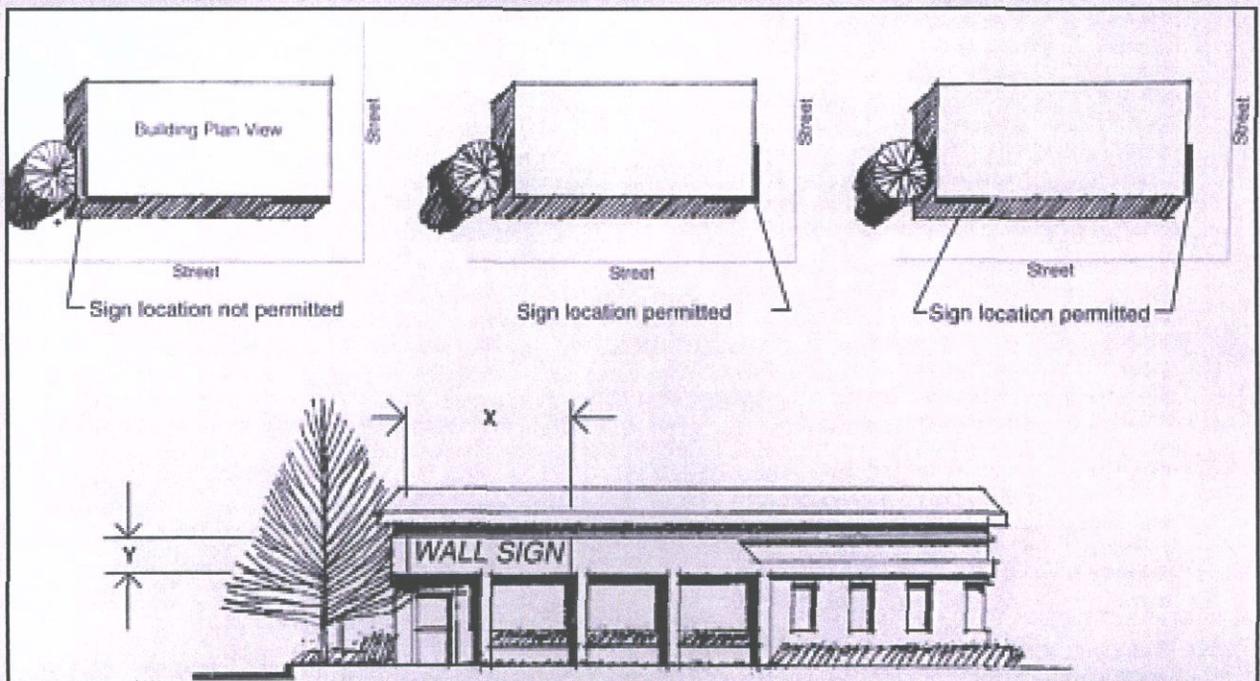


FIGURE 4-13
TYPICAL SIGNAGE LAYOUT



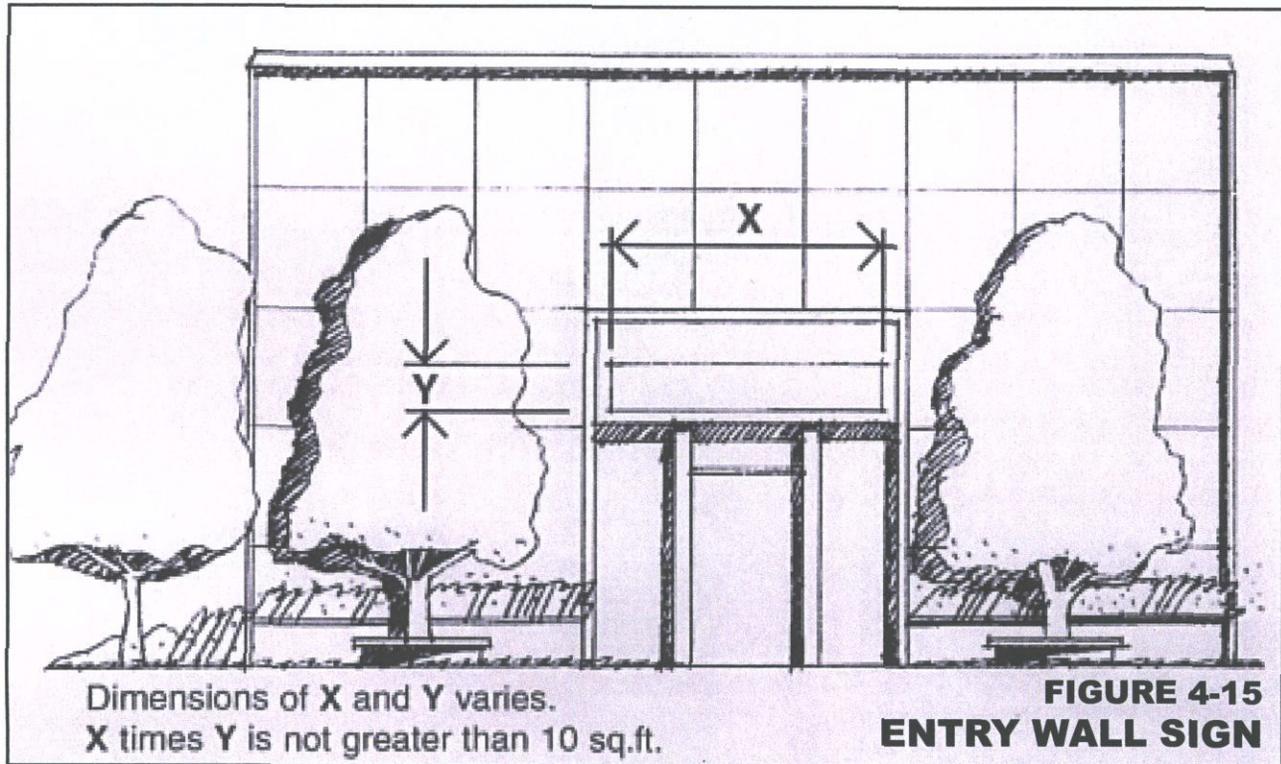
Dimensions of X and Y varies. X times Y is not greater than 100 sq. ft. for industrial projects and 150 sq. ft. for commercial projects.

FIGURE 4-14
WALL SIGN LOCATION EXAMPLES

Review, as required under Section 6.1 of the BIDS Specific Plan. All new signage shall comply with the following policies:

4.5.1 Wall Signs

To ensure consistency throughout the BIDS Specific Plan, wall signs shall conform to the following:



- Wall signs and logos attached to the building shall be individual letters and surface mounted.
- No signs shall be boxed with internal lighting and attached to the building.
- Wall signs shall be located on the building for optimum visibility from the adjacent street or parking area.
- Wall signs shall not exceed an area equal to one square foot for each lineal foot of building frontage.
- The maximum sign size for a building façade adjacent to a street or parking area shall be one-hundred (100) square feet for office and industrial projects, one-hundred (150) square feet for commercial projects.

**TABLE 4-1
WALL SIGN CHART**

Land Use	Maximum Number	Maximum Area Per Sign	Other
Commercial	1/Building face	150 sq. ft.	Below roof line Raceways not permitted Channel letters only
Office	1/Business	100 sq. ft.	
Business Park	2/Business/street frontage	100 sq. ft.	

**TABLE 4-2
FREESTANDING SIGN CHART**

Land Use	Maximum Number	Maximum Area Per Sign Face	Maximum Height	Other
Commercial	1/250 ft.	100 sq. ft.	7 ft.	Multi-tenant panels are permitted
Business Park and Office	1/building	50 sq. ft.	7 ft.	Multi-tenant panels are permitted

NOTE: (1) Entry signs, information/directional signs and temporary signs are subject to separate regulations.

(2) Existing signs within the BIDS campus are deemed permitted and may also be relocated or modified.

- Wall signs shall be internally illuminated or non-illuminated.
- Wall signs shall be limited for secondary tenants to a maximum of one (1) sign per tenant. In no event shall there be more than four (4) secondary wall signs permitted per building and no more than two (2) per building elevation.

4.5.2 Freestanding Signs

Buildings may have freestanding signs for project identification subject to the following policies:

- Freestanding signs for business identification shall be limited to perimeter locations adjacent to existing arterials and shall be of a monument design. These signs shall not exceed seven (7) feet in height, as measured from the adjacent grade, and not more than fifty (50) square feet in area for industrial and office uses and one hundred (100) square feet for commercial uses.

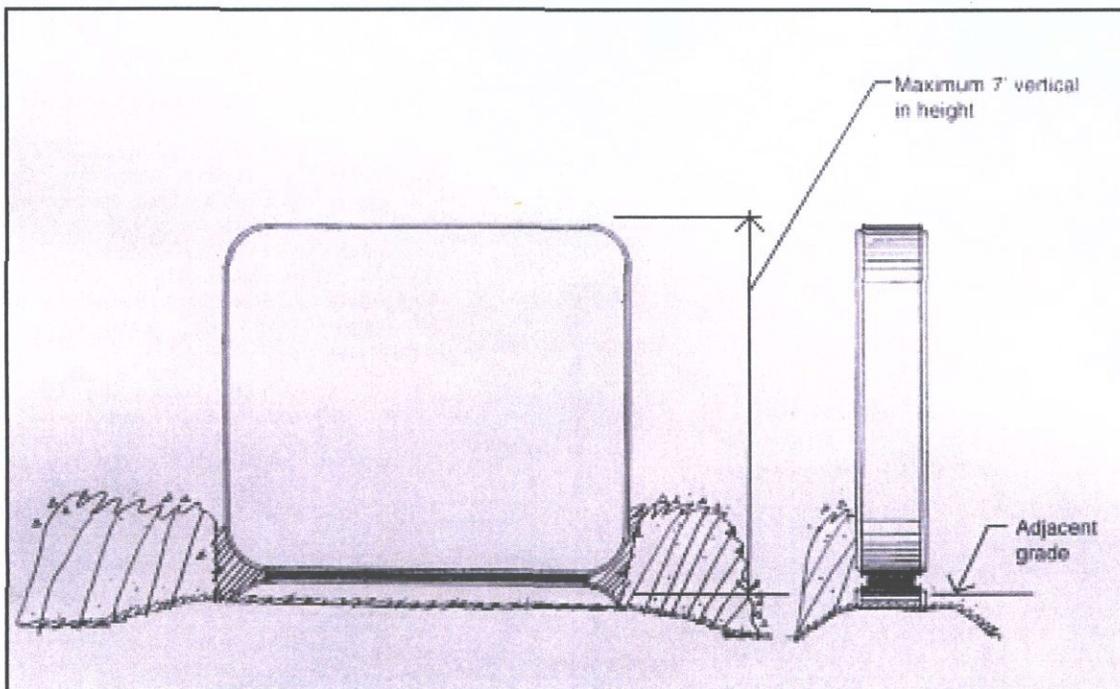
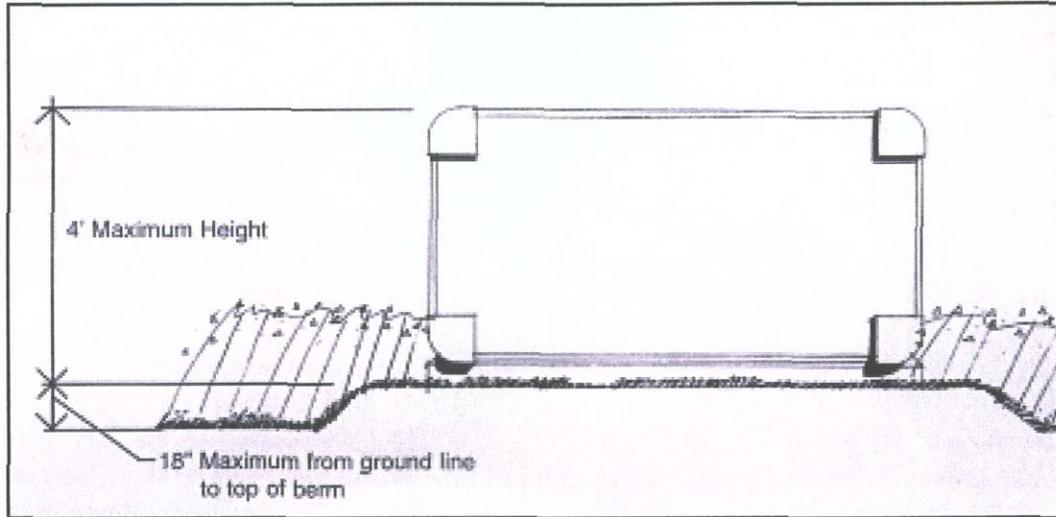


FIGURE 4-16
FREESTANDING BUSINESS IDENTIFICATION SIGN

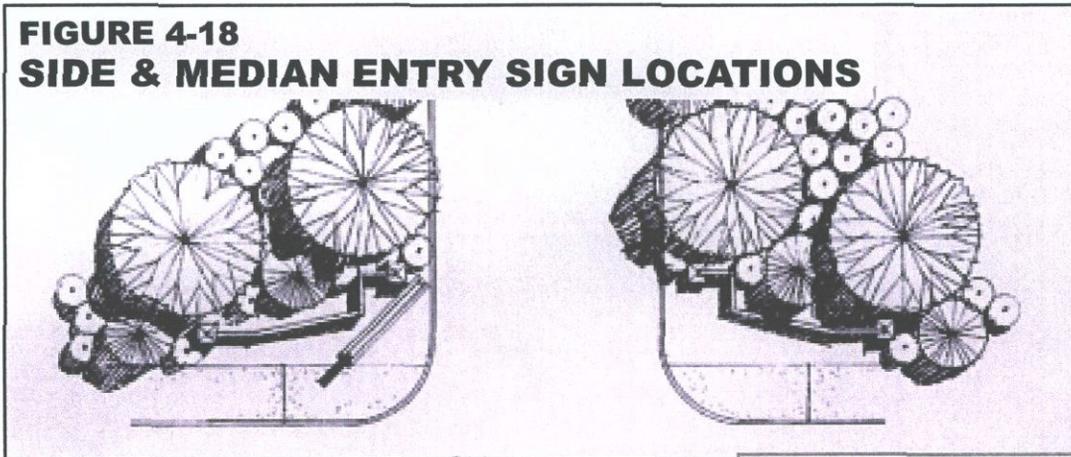
- Freestanding signs for tenant identification may be installed within or adjacent to a private entry driveway. Entry signs shall be located and sized to not interfere with vehicular visibility and/or movement. Entry signs shall be limited to thirty-two (32) square feet and shall not exceed four (4) feet in height. Entry signs may be placed on a berm not exceeding eighteen (18) inches in height. Entry signs shall include the building address.



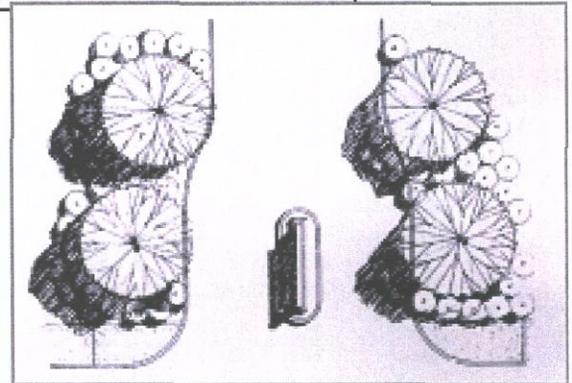
**FIGURE 4-17
FREESTANDING TENANT IDENTIFICATION SIGN**

- Center identification signs may also be installed at key intersections, in landscaped areas within the right-of-way or landscaped street medians. Center identification signs shall be limited to thirty-two (32) square feet and shall not exceed four (4) feet in vertical height, and shall only identify the overall business park.

**FIGURE 4-18
SIDE & MEDIAN ENTRY SIGN LOCATIONS**



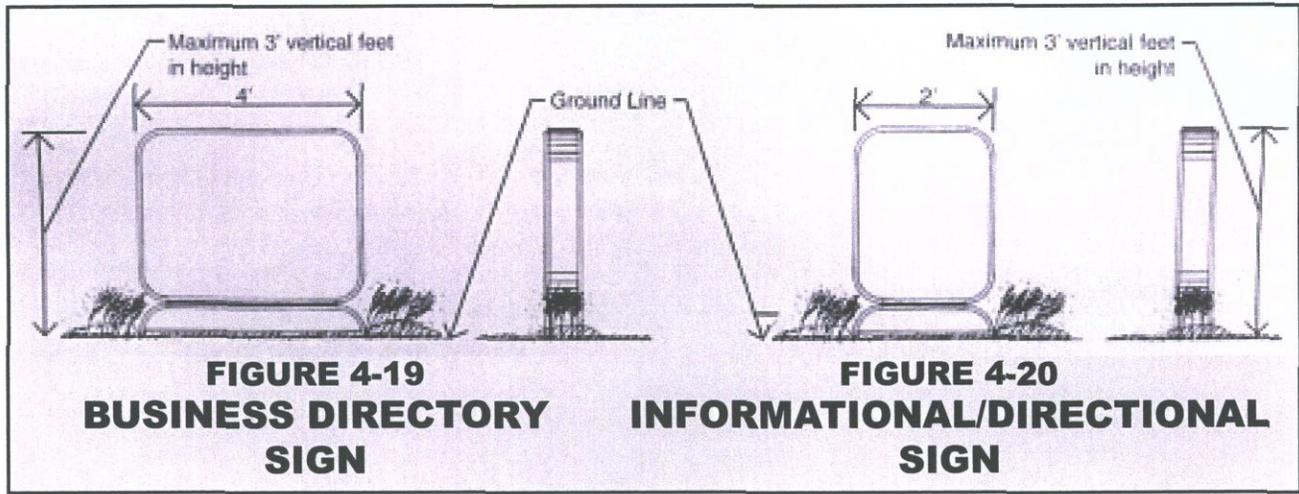
- All freestanding signs shall be of a monument design, including: business identification, business directory, and information/directional identification signs. Street side signs, at access driveways, shall be used to identify a building address/tenant, and to direct traffic to that building. Freestanding signs may be illuminated using City-approved directional lighting.



4.5.3 Business Directory and Information Signs

Business Directory and Information Signs are permitted subject to the following policies:

- Business directory signs that are freestanding shall be located near the primary entry and access drive. These signs shall be limited to identifying the building address and tenants, and shall be visible from the intersection of a private driveway and an adjacent street. These signs shall not be allowed elsewhere within the landscape setback area or along street frontages.

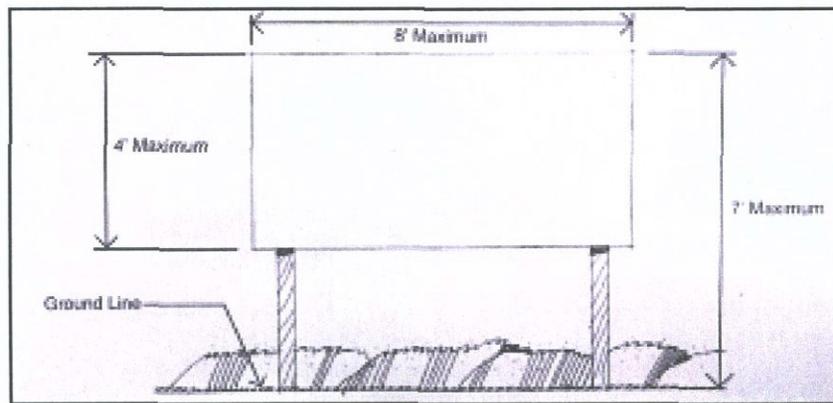


- Business directory signs shall not exceed a maximum area of twelve (12) square feet per face, may be double faced adjacent to a street front and single faced adjacent to a building, and shall not exceed three (3) feet in vertical height. Signs shall be limited to one sign per building. More than one building address and tenant may be identified on one sign. Signs shall be consistent with the overall building architecture.
- Information / directional signs shall be used to provide direction to on-site automobile traffic or pedestrians and shall not be visible from off-site areas. Informational / directional signs shall be limited to six (6) square feet per face, double faced. These signs shall not exceed three (3) feet in vertical height.
- Informational / directional signs shall be limited to the identification of function and / or service and shall not contain the name of the business, company or corporation providing the function and / or service. When appropriate, such signs shall contain a directional arrow and provide direction to functions and / or services or information such as Authorized Vehicles Only, Handicapped Parking Only, and Loading Zone.

4.5.4 Temporary Signs

Temporary signs are permitted subject to the following policies:

- Temporary signs shall be allowed to provide information and facilitate information during the construction and marketing process. Such signs shall be limited to one per lot, with a maximum of thirty-two (32) square feet and seven (7) feet in overall height.
- Temporary directory signs shall be permitted on construction sites, and limited to one (1) for all contractors. The sign shall not exceed thirty-two (32) square feet, unless legally required by government contracts to be larger. The sign shall not exceed seven (7) feet in overall height and shall be located no less than ten (10) feet from any property line. These signs shall be removed upon completion of the project.



**FIGURE 4-21
TEMPORARY SIGN**

- Future tenant signs may be placed on vacant or developing property to advertise the future use of the property and where further information may be obtained. Such signs shall be limited to one per street frontage and to a maximum of thirty-two (32) square feet in area and seven (7) feet in overall height. These signs shall be placed no less than ten (10) feet from the property line. Any such sign shall be removed upon occupancy of the project.

4.6 SITE LIGHTING GUIDELINES

Lighting is intended to create a nighttime character that reinforces the image of the BIDS Specific Plan as a quality business location. All exterior lighting is to be generally consistent in height, spacing, color and type of fixture throughout the BIDS Specific Plan area. Exterior lighting is to be provided to enhance the safety and security of motorists, pedestrians and cyclists. Exterior lighting includes lighting for parking areas, vehicular and pedestrian circulation, building exterior, service areas, landscaping, security and special effects.

4.6.1 General On-Site Lighting Parameters

Lighting shall meet all requirements of the City of Seal Beach. The following elements should be considered:

- All exterior on-site lighting should be shielded and confined within site boundaries. No direct rays are permitted to shine onto public streets or adjacent lots.
- Lights mounted to the roof parapet are not permitted. Wall-mounted light fixtures used to illuminate parking lots are not permitted.
- Lighting shall create a sequence of varying illumination levels leading up to the building entrance. This would include the orchestration of light from parking lighting, to pedestrian lighting, special feature lighting, and lighting from within.

4.6.2 Vehicular Circulation and Parking Area Lighting

All vehicular circulation and parking lot lighting should have zero cut-off fixtures (i.e. lens is not visible from an angle).

Pole height should be as follows:

- Vehicular Circulation: twenty-five (25') feet maximum
- Parking Area : twenty (20') feet maximum

4.6.3 Pedestrian Lighting

Pedestrian walkways and building entries should be illuminated to provide for pedestrian orientation and to clearly identify a secure route between parking areas and points of entry to the building. Walkway lighting should have zero cut-off fixtures mounted at a uniform height no more than sixteen (16') feet above the walkway. Building entries may be lit with soffit, bollard, step or comparable lighting.

- Step or bollard lighting should be used to clearly illuminate level changes and handrails for stairs and ramps.
- Courtyards, arcades and seating areas should be lighted to promote pedestrian use and safety. A variety of lighting may be used to create interest and special effects in coordination with the character and function of the area.
- Pedestrian lighting shall be subdued warm-white mercury or incandescent lamps.

“Lighting shall create a sequence of varying illumination levels leading up to the building entrance. This would include the orchestration of light from parking lighting, to pedestrian lighting, special feature lighting, and lighting from within”

4.6.4 Security Area Lighting

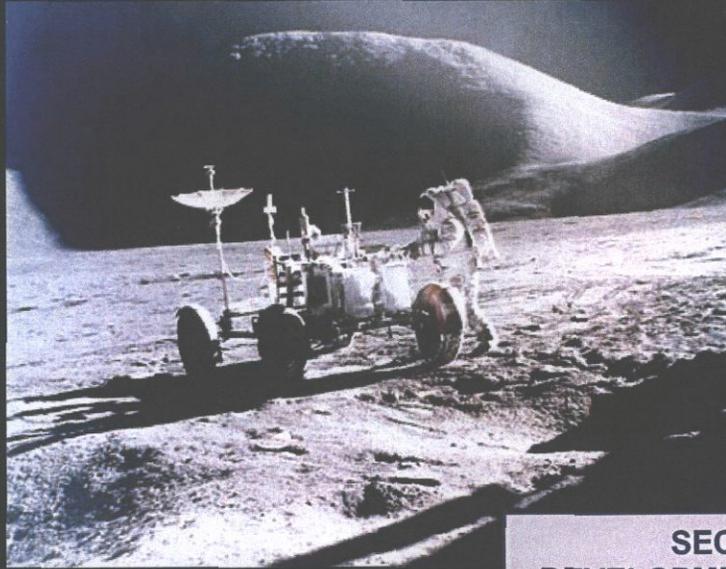
Service area and security lighting should be visible only within the limits of the service area. Wall-mounted, security-type, service area lighting fixtures may be used only in screened service areas and only if direct lighting and glare is kept within these areas. In all other areas, wall-mounted service lighting should consist of cut-off type fixtures. Service area and security lighting may not be substituted for pedestrian, architectural or parking area lighting.

4.6.5 Accent Lighting

Unique lighting may be used to feature architectural elements, landscaping, entries and pedestrian areas, provided it is compatible with all other lighting. Accent lighting used in landscaping and pedestrian areas should employ light sources such as metal halide, quartz or mercury lamps in order to accurately render plants and lawn colors.

4.6.6 Architectural Lighting

All exterior architectural lighting should utilize indirect or hidden lighting sources. Acceptable lighting includes wall washing, overhead down lighting and interior lighting that spills outside. Entry areas should be lit so as to provide an inviting glow from the interior.



**SECTION FIVE
DEVELOPMENT REGULATIONS**

BOEING
INTEGRATED DEFENSE SYSTEMS
SPECIFIC PLAN
SEAL BEACH SITE

Boeing Realty Corporation



SECTION FIVE

DEVELOPMENT REGULATIONS

5.1 PURPOSE AND INTENT

These regulations are the standards for development within the BIDS Specific Plan area and will act as the controlling mechanism in implementing development. Implementation of the provisions set forth in this section will ensure that the project will proceed in a coordinated manner, consistent with the goals and policies of the BIDS Specific Plan and the City of Seal Beach General Plan, as implemented by this document.

The regulations are pursuant to Article 8, Authority and Scope of Specific Plans of the Planning and Zoning Law of the Government Code and are in compliance with the provisions of Sections 65450 of the Government Code, and Chapter 28, Article 29.5 of the Code of the City of Seal Beach.

5.2 DEVELOPMENT REGULATIONS POLICIES

The following policies shall apply to all developments and land uses within the BIDS Specific Plan:

- 5.2.1 Unless otherwise specified, all development within the BIDS Specific Plan area shall comply with the Code of the City of Seal Beach. Terms used herein shall have the same meaning as defined in the Code of the City of Seal Beach unless otherwise defined herein.
- 5.2.2 Any standards, uses, details or issues not specifically covered by the BIDS Specific Plan shall be subject to the regulations of the Code of the City of Seal Beach. Wherever there is inconsistency between the development standards of the Code of the City of Seal Beach and the development standards of the BIDS Specific Plan, those in the Specific Plan shall control, pursuant to Section 28-1702 of the Code of the City of Seal Beach.
- 5.2.3 Development standards, regulations and procedures in the BIDS Specific Plan that reference the Code of the City of Seal Beach shall be those in effect on the date the application for Vesting Tentative Tract Map 16375 has been determined to be complete pursuant to Section 65943 of the Government Code.
- 5.2.4 Whenever a use has not been specifically listed as being a permitted use in a particular Planning Area of the Specific Plan, it shall be the duty of the Planning Commission to determine if the use is consistent with the intent of this Specific Plan and compatible with other permitted uses.

- 5.2.5 Permitted land uses also include accessory buildings and uses that are customarily incidental to, and located on the same site with, the main building.
- 5.2.6 The Planning Commission may determine that other conditional uses not specifically identified are conditionally permitted uses. The conditional use permit process is described in Section 6.2.
- 5.2.7 All construction within the boundaries of the BIDS Specific Plan shall comply with the provisions of the Uniform Building Code and the various mechanical, electrical and plumbing codes adopted and locally modified by the City of Seal Beach, as established by Section 5 of the Code of the City of Seal Beach.
- 5.2.8 If any regulation, condition, program or portion thereof of this Specific Plan is for any reason held invalid or unconstitutional by any court of competent jurisdiction, such portion shall be deemed a separate, distinct and independent provision and the invalidity of such provision shall not affect the validity of the remaining provisions hereof.
- 5.2.9 An EIR (02-1) which analyzes the worst-case situation for the cumulative impacts of physical development as proposed by the Specific Plan has been certified by the City Council and is referenced hereby.
- 5.2.10 Request for expansion and/or new construction of Boeing Space & Communications buildings and facilities within Planning Area 1 for BIDS-related uses shall be regulated by the Precise Plan Review process as described in Section 6.1.
- 5.2.11 Planning Area 4 is planned for commercial lodging and retail land uses that provide commercial and visitor-serving services. For these uses, PA-4 is regulated by design guidelines, development regulations and requirements identified for Planning Area 4. If, after 18 months from the adoption of the BIDS Specific Plan, such lodging and commercial land uses are determined infeasible, this Planning Area may be developed with business park land uses.
- 5.2.12 Temporary buildings required for BIDS uses shall be permitted subject to review and approval by the Planning Commission under the Precise Plan Review process as described in Section 6.1.
- 5.2.13 All structures, facilities and uses in existence within the Specific Plan area at the time of adoption of the Specific Plan shall be deemed to be in conformance with the development regulations, land use plan, public facilities and services plans, design guidelines and other applicable provisions of this Specific Plan.

- 5.2.14 The overall intensity for the Specific Plan area has been established with a maximum Floor Area Ratio (F.A.R.) The F.A.R. for each Planning Area is identified in Table 5-2.
- 5.2.15 F.A.R. is defined as the ratio between the amount of gross floor area permitted to be constructed on a legal building lot and the size of the lot. In computing gross floor area of a building, the gross area confined within the exterior walls of the building shall be considered as the floor area of each floor of the building. This includes space devoted to hallways, stairwells, elevator shafts, lobbies, light courts and basement storage. Gross floor area does not include covered parking floor space with necessary interior driveways and ramps thereto, space within a roof structure or penthouse for the housing of equipment or machinery incidental to the operation of the building, and space for loading and storage of helicopters.
- 5.2.16 Large-lot subdivision, for the purpose of financing or conveyance, may be approved provided the Subdivision Map related thereto includes a declaration that the lots created are not building sites. Installation or bonding of infrastructure improvements shall not be made a condition of approval of a large-lot subdivision unless for financing or conveyance purposes infrastructure improvements are necessary to provide required street frontage access.
- 5.2.17 Permits and approvals required from other agencies may necessitate revisions, adjustments and/or amendments to the BIDS Specific Plan. The City of Seal Beach shall not unreasonably withhold approval of any revision, adjustment or amendment that is mandated by conditions of approval imposed by any other governmental agency.

5.4 PARKING

All developments will be required to meet the minimum parking standards as established by the City of Seal Beach Municipal Code.

In addition, the following parking standards outlined on the page 5-4 shall apply:

1. Standard parking stall size shall be nine (9) feet wide by nineteen (19) feet deep in Planning Areas 1, 2 and 3. In Planning Area 4, standard parking stalls shall be (9) feet wide by twenty (20) feet deep. Parking stalls may be reduced to provide a wheel stop at seventeen (17) feet and eighteen (18) feet, respectively, with a two (2) foot overhang to expand the landscaping. This additional landscape area will not be credited toward the required landscape percentage.

2. A compact parking stall size of eight feet zero inches (8'-0") wide by seventeen (17) feet deep may be proposed for up to a maximum of twenty five (25) percent of the total required parking spaces in each Planning Area.
3. Shared parking may be permitted for combined office, research and development, commercial and industrial uses. A shared parking program may allow for a reduction of required parking by up to twenty five (25) percent, based upon a shared parking analysis. Shared off-site parking facilities may also be permitted for adjacent office and industrial uses. A shared parking program may be approved under the provisions of the Conditional Use Permit process as described in Section 6.2.
4. Handicapped accessible parking spaces shall be provided as required by law.
5. Any existing BIDS required surface parking in Planning Area 1 removed for new development, shall be replaced on a one for one basis concurrent with such new development. If new development requires a greater number of parking spaces, the increase shall also be provided.
6. The parking standards for self-storage uses are not defined in the City of Seal Beach Municipal Code. Parking standards for this use shall be 1 space per 5,000 square feet of single story development, plus 1 additional space per 2,000 square feet for second story development. In addition, the management office shall provide a minimum of two spaces.

5.3 DEVELOPMENT STANDARDS

The development standards for the BIDS Specific Plan are established to provide a quality business park in conjunction with the potential for hotel and commercial uses, within the context of existing BIDS operations and facilities.

All Planning Areas are permitted to be developed with business park land uses. However, PA-4 provides additional flexibility for commercial uses, including hotel, retail, restaurant, and other commercial land uses. The permitted and conditionally permitted land uses are in Table 5-1. Table 5-2 identifies the development standards for each Planning Area.

DEVELOPMENT REGULATIONS

**TABLE 5-1
BIDS SPECIFIC PLAN PERMITTED LAND USES**

LAND USE	PLANNING AREA			
	1	2	3	4
BUSINESS PARK				
Boeing Integrated Defense Systems, including all existing buildings, facilities and operations, and associated governmental facilities	✓	✓	✓	✓
Manufacturing	✓	✓	✓	✓
Warehousing and Distribution	✓	✓	✓	✓
Light Industrial	✓	✓	✓	✓
Research and Development	✓	✓	✓	✓
Point of Sale Industrial/Manufacturing	✓	✓	✓	✓
Self Storage Facilities (indoor)			✓	
Laboratories	✓	✓	✓	✓
Communication Facilities	✓	✓	✓	✓
Parking Structures	✓	✓	✓	
OFFICE				
	✓			
COMMERCIAL				
Banks and other Financial Institutions				✓
Prescription Pharmacies				✓
Restaurants				✓
Hotels and Motels (with or without conference facilities)				✓
General Retail and Commercial Services				✓
USES SUBJECT TO A CONDITIONAL USE PERMIT				
LAND USE	PLANNING AREA			
	1	2	3	4
Heliports, Maintenance and Service Facilities	✓	✓		
Automobile service center			✓	
Medical Facilities	✓	✓	✓	✓
Ice skating, ice hockey, gymnasium, and other similar facilities for sports related activities		✓	✓	
Conference facilities	✓	✓	✓	
Restaurant	✓	✓	✓	
On-sale and off-sale liquor establishments	✓	✓	✓	✓
Restaurants offering drive-in service, outdoor dining and/or entertainment				✓
Commercial/retail services with drive-in service				✓
Offices		✓	✓	✓
Parking structure				✓
Retail uses associated with warehouse/showroom	✓	✓	✓	

NOTES: (1) Self-storage is a permitted land use only within Planning Area 3 on lot 6 of Vesting Tentative Tract Map 16375.
 (2) Automobile service center is a conditionally permitted land use only within Planning Area 3 on lot 6 of Vesting Tentative Tract Map 16375.
 (3) Business Park land uses are permitted within Planning Area 4 if, after thirty-six (36) months from the issuance of a Coastal Development Permit, such lodging and commercial land uses are determined infeasible.

DEVELOPMENT REGULATIONS

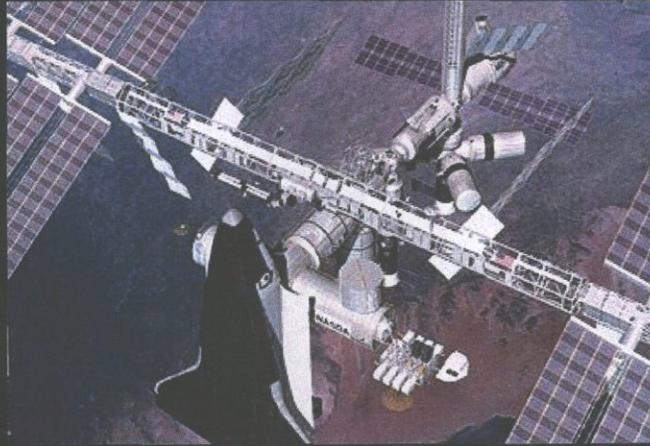
**TABLE 5-2
DEVELOPMENT STANDARDS**

STANDARDS	PLANNING AREA			
	1	2	3	4
Minimum Lot Size (sq. ft.)	10,000	10,000	10,000	7,000
Minimum Lot Depth/Width	100'/100'	100'/100'	100'/100'	100'/70'
Maximum Building Height, Main Building (10 acres or larger site)	75'	75'	75'	N/A
Maximum Building Height (less than 10 acre site)	40'*	40'*	40'*	40'*
Maximum Intensity (F.A.R.)	0.75	0.60	0.60	0.65
Maximum Lot Coverage	70%	70%	70%	60%
Minimum Setback				
Front	25'	25'	25'	15'
Interior Side	10'	10'	10'	15'
Exterior Side	15'	15'	15'	15'
Rear	10'	10'	10'	15'
Adjacent to Westminster Avenue/ Seal Beach Boulevard	35'	35'	35'	35'
Minimum Landscape	10%	10%	10%	10%
Minimum Perimeter Landscape				
Front	15'	15'	15'	10'
Interior Side	5'	5'	5'	5'
Exterior Side (street side)	10'	10'	10'	10'
Interior Rear	5'	5'	5'	5'

* NOTE: Architectural projections and screening of mechanical equipment are permitted. These features shall not exceed seven (7) additional feet.

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**SECTION SIX
IMPLEMENTATION PROCEDURES**

**BOEING
INTEGRATED DEFENSE SYSTEMS
SPECIFIC PLAN
SEAL BEACH SITE**

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SECTION SIX

IMPLEMENTATION PROCEDURES

6.1 PRECISE PLANS

The BIDS Specific Plan shall be implemented through the use of Precise Plans. Precise Plans establish development guidelines for proposed structures or improvements for each planning area within the Specific Plan for the following reasons:

1. To ensure consistency with the adopted BIDS Specific Plan.
2. To encourage continuity in design and development.
3. To promote the highest contemporary standards of site design.
4. To assure substantial long range compliance with the City of Seal Beach General Plan.

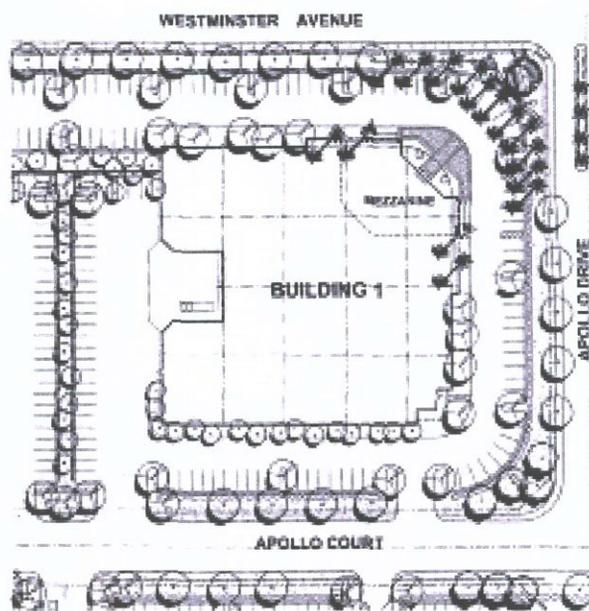
Procedures for review of Precise Plans shall be in accordance with the provisions of Article 29 of Section 28-2900 through Section 28-2910 of the Code of the City of Seal Beach, as established by Ordinance 948. Exceptions are noted below:

1. The fee for the Precise Plan review shall be in accordance with the current comprehensive fee resolution of the City of Seal Beach. Additional funds may need to be deposited with the City of Seal Beach at the request of the Director of Development Services. This may be required to ensure full cost recovery of City services in reviewing Precise Plans and other discretionary land use requests.
2. Along with the supplemental requirements of Article 29, Section 28-2903, at the request of the Director of Development Services, the developer shall submit:
 - a) A preliminary landscape plan, addressing streetscape and project landscaping themes, including proposed size and quantities of all proposed landscape materials.
 - b) Architectural elevations and floor plans drawn to scale, including samples of exterior materials and textures.

- c) Detailing of exterior hardscape materials and textures.
- d) Precise sign plan locations, size, colors and letter type of all proposed signage.
- e) Preliminary grading plan, showing areas of cut and fill, location and elevation of all pads, and height of cut and fill.
- f) Such other information as may be needed and deemed appropriate by the Department of Development Services (e.g., traffic studies, noise studies, water quality management plans, etc.).

Precise Plans shall be required prior to obtaining building permits for all Planning Areas within the BIDS Specific Plan. Precise Plans shall be processed concurrent with discretionary approvals to the greatest extent practicable.

Authority for approval of Precise Plans shall rest with the Planning Commission. Precise Plan approval shall be valid for a period of two (2) years. If construction of a project does not commence within that time period and proceed with due diligence thereafter, the Planning Commission may grant a time extension of up to two years. If, after 4 years from the date of initial approval of a Precise Plan, construction does not commence, the approval of the Precise Plan shall terminate and additional review and approval will be required by the Planning Commission. In the event of litigation, time periods for approval shall be tolled for the period of time litigation is in existence.



**FIGURE 6-1
CONCEPTUAL
LANDSCAPE PLAN DETAIL**

6.2 CONDITIONAL USES AND VARIANCES

Applications for all conditional use permits within and variances from the BIDS Specific Plan shall be prepared and processed in accordance with Article 25 of the City of Seal Beach Municipal Code.

6.3 SUBDIVISION MAP FILING AND REVIEW PROCEDURES

All subdivisions within the BIDS Specific Plan must comply with Chapter 21 of the City of Seal Beach Municipal Code and the State of California Subdivision Map Act.

6.4 SPECIFIC PLAN REVISIONS AND ADJUSTMENTS

6.4.1 Administrative Revisions

Revisions to the Specific Plan which serve to implement the intent of the Specific Plan are permitted through the administrative revision process. This section grants authority to the Planning Commission to act on requests for administrative revisions to implement the Specific Plan pursuant to the Consent Calendar Plan Review process set forth in Section 28-2407.B of the Seal Beach Municipal Code.

The following types of revisions are permissible through the administrative revision process:

- The addition of information to the exhibits or text which serve to clarify, but do not change, its meaning or intent.
- Modifications to the alignment of the Planning Area boundaries to coincide with specific development plans, as recommended by the Planning Commission.

All administrative revisions are to be reviewed for compliance with the purpose, intent and objectives of the BIDS Specific Plan.

6.4.2 Administrative Adjustments

Minor adjustments to development standards that constitute a reasonable use of property and do not endanger the public health, safety or welfare are permitted through the administrative process. This section grants authority to the Planning Commission to act on requests for administrative adjustments to implement the Specific Plan pursuant to the Consent Calendar Plan Review process set forth in Section 28-2407.B of the Seal Beach Municipal Code. Administrative adjustments may allow the following:

1. A decrease of not more than 10 percent of any required building setback.
2. A decrease in not more than 10% of the required parking spaces, with a parking study approved by the Planning Commission.
3. Zero-lot line development for interior side and interior rear yards.

6.5 SPECIFIC PLAN AMENDMENTS

The procedures for amending the BIDS Specific Plan will be in accordance with Government Code Section 65453 and Article 29.5 of Chapter 28 of the Seal Beach Municipal Code. Prior to approving any amendment to the BIDS Specific Plan, the City Council shall make the following findings:

1. The amendment is compatible with the purpose, intent, spirit and goals of the BIDS Specific Plan.
2. The amendment is consistent with the City of Seal Beach General Plan.
3. The amendment is consistent with the City of Seal Beach Local Coastal Plan, as certified by the California Coastal Commission (if applicable).
4. The amendment is compatible with adjacent land uses.

In the event that an amendment to the BIDS Specific Plan would render a Precise Plan inconsistent with the amended Specific Plan, such previously adopted Precise Plan shall be concurrently amended to be in conformance with the amended Specific Plan.

6.6 FINANCING

In order for the project to be fiscally self-sufficient, the following options can be considered for implementation.

1. A lighting and landscape district can be implemented in order to defray the costs associated with street maintenance and street light costs.

Additionally, a variety of other financing options can be considered by the City of Seal Beach for financing facilities and services to be provided within the BIDS Specific Plan. These include:

1. Establishment of a Mello-Roos Community Facilities District, which enables governmental entities to finance public infrastructure on a tax-exempt basis.
2. Special assessment districts.

3. Benefit assessment financing (generally reserved for drainage, flood control and street lighting).
4. Integrated financing districts (i.e. landowner assessments that are contingent upon development).

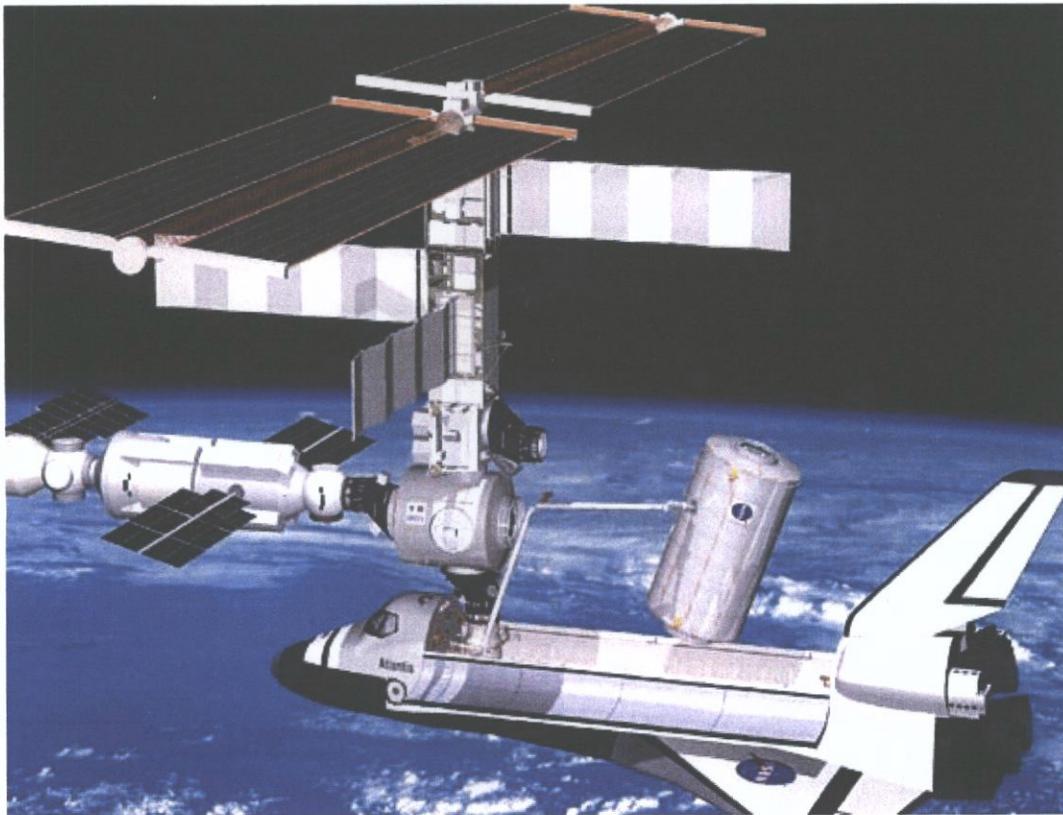
All special district financing mechanisms must have approval of the City Council prior to adoption. As conceived, establishment of a district would enable the City to levy a special tax, and to issue bonds for the financing of the improvements and services. Among the facilities that potentially could be financed are flood control, drainage and utilities, street improvements and other infrastructure and related facilities. The district provisions for those services would cover all phases, including planning, design, acquisition, construction, operations and maintenance. These assessments would only affect the BIDS Specific Plan area. BIDS and/or other landowners within the Specific Plan area will retire the debt incurred.

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